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# NARROW GAUGE AND SHORT LINE GAZETTE

NOVEMBER/DECEMBER 2022

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## COVER

The stillness of a winter night in the mountains of New Mexico is shattered by the sounds of a pair of K-36 locomotives as they drag a freight train to the summit of Cumbres Pass. After 45 years working as a watercolor artist, Jan Rons has now retired from fulltime painting. He does however enjoy sharing his art when it is possible.



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P.O. Box 48 F Bucklin, MO 64631  
877-787-2467 F 660-695-4433  
info@whiteriverproductions.com  
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PUBLISHER  
Kevin EuDaly

EDITOR  
Robert W. Brown, MMR  
gazette@att.net  
650-948-9361

EDITORIAL ADDRESS  
Robert W. Brown  
P.O. Box 26, Los Altos, CA 94024

SUBSCRIPTION INQUIRIES  
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ART DIRECTION / PAGE LAYOUT  
Sharon Olsen, Just SO Graphic Design

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Mike Lindsay  
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DEALER SALES  
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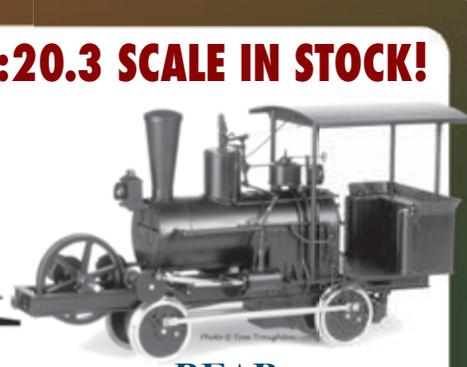
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### AUTHORS

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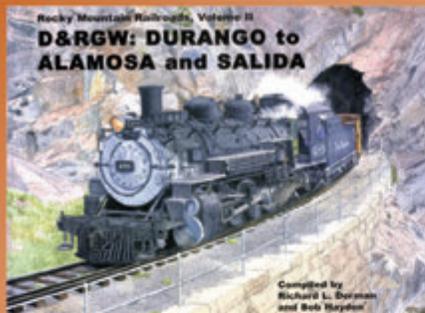
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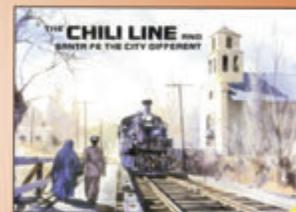
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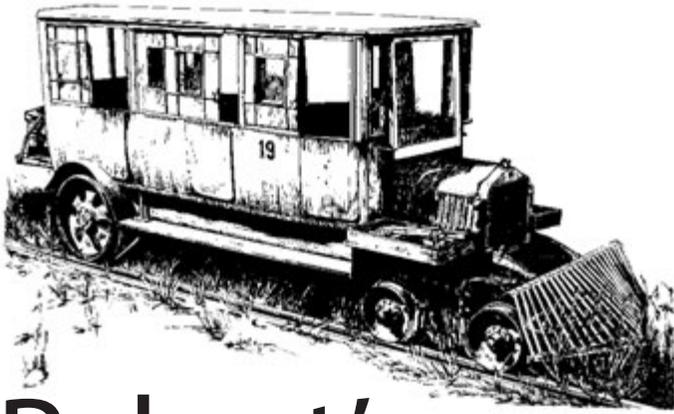
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# Robert's Ramblings

## CARDBOARD???

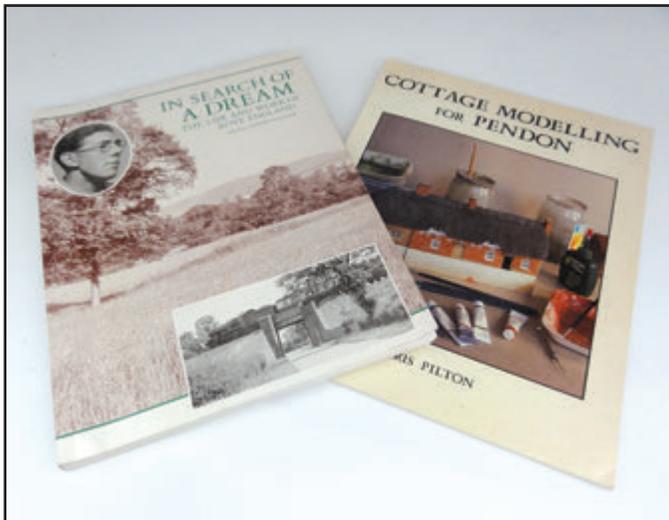
I have just finished reading the late Roye England's book *In Search of a Dream, The Life and Work of Roye England*. Edited by Stephen Williams and published by Wild Swan Publications. It is undated, but copies are available on the internet.

Roye England was born in Australia. While in his teens in the 1920s, he invented a mechanical device for operating a model railroad automatically and decided to go to England to patent it. The device was never made nor marketed, but when Roye arrived in England, he fell in love with the Vale of the White Horse, located in the Cotswold area of central south-west England. He was horrified to see the asphaltting over, and bulldozing under, of the beautiful thatch roof, stone cottages, farms, and lanes of the area, and began trying to preserve it in photos and sketches.

Then he decided to someday build a museum with scale models of the Vale, and this book describes his lifelong work to produce The Pendon Museum near Oxford.

Irene and I have visited Pendon three times including a tour by Roye. There is no better modeling, anywhere. It's all in 4mm scale and I remember looking in the window of a little cottage and seeing socks drying on the hearth and a tiny crystal set with earphones on a table. I urge you to Google Pendon Museum for a series of excellent videos. The cottages are made from cardboard with hand-scribed bricks and stones, colored with watercolors. The flowers have separate pedals, and the ivy on the walls separate leaves. All in 4mm scale. One cottage has 60,000 hand-cut, painted and weathered roof slates, installed one by one.

Not all the models were made by Roye, but you have to audition to be a Pendon builder by building an acceptable privy. One of the accepted modelbuilders was Chris Pilton, who wrote a book in 1987 titled *Cottage Modelling for Pendon* (Wild Swan Publications). I see it is still available on the internet. I have just finished reading Chris' book and decided to try some of his techniques on my latest Bodie model of the McKenzie Brewery with its Lady's Parlor, drawn by Neil Pfaffman, in the January/February 2020 *GAZETTE*. Seems the ladies of Bodie preferred to drink their brew alone. But what kind of card stock would work best? I searched the internet and found Illustration Boards (#300 Illustration Board). It can be ordered on Amazon in 9- x 12-inch 3-packs, but is also



Above: These two books are well worth reading.

Right: Just a sample of a Pendon scene.



available elsewhere in 5x7, 8x10, 8½x11, 9x12, and 11x14-inch 3-packs. I ordered several 3-packs of 9x12 boards from Amazon. I found this cardboard perfect. The small size is a virtue on a small work surface. It's about 1/16-inch thick, has a mat finish, light manila colored surface, cuts easily and takes scribing and paint well. But it is pricy. Three 9x12 pieces cost around \$13.00.

The brewery shown here was made from Illustration Board except for the trim, sidewalk, porch, and Wild West shakes, chimneys, and Tichy doors and windows. My brewery is nowhere near a Pendon model, but I am looking forward to trying some scribed-on bricks and stones.

**P**

Please do look at that Pendon website, I know it will inspire you.

I could not attend the 42<sup>nd</sup> National Narrow Gauge Convention held at the Murano Hotel in Tacoma, Washington, September 1-4, 2022; however Dave Adams was there and graciously provided the following summary:

Robin Peel and the convention committee continued the tradition of fine conventions to the delight of 550 paid registrants, spouses and volunteers resulting in the printing of 750 name badges. The format followed past conventions with a twist in the layout tour times and close of the vendor room was one hour earlier to help avoid the worst of the automobile traffic in the area. This worked well, and the group I was layout touring with visited more layouts than we had in 2012. Even so, with 34 layouts open for tours, it simply was not possible to see them all.

Five clinic rooms each offered two clinics in the morning and the evening covering 36 different subjects, with most clinics being presented twice.

The contest room contained models and photos that were all excellent, and worth studying. It is truly unfortunate that the travel climate precluded the room from being filled to capacity. See the Gallery on page 44 for a listing of the contest winners and photos of First Place Awards.

The Exhibition Hall hosted 42 vendors and 3 really nice modular layouts without feeling crowded. The lobby of the



Above: My McKenzie Brewery with Ladies Parlor.

hall hosted the registration desk, the Hangman's Creek logging railroad, coffee and pastries, and on the last night, an ice cream social.

The closing meeting was well attended, and the Chair made sure the committee, the volunteers, layout owners and clinicians were recognized for their contributions to the success of the convention. The contest winners were announced including Best of Show to Brian Block, and Mt. Albert Scale Lumber made an award to the model they determined to make the best use of scale wood. Inter-Action Hobbies donated prizes for a drawing, and a number of people went home with something they did not expect.

The assembled group then heard about future convention plans from Denver, Pittsburg, St. Louis and a bid from



Above: One of the many clinics at the convention. Photo by J. Sauer.



Right above: The Moose Creek Lumber display in On30. Photo by J. Sauer.



Right: The Monson & Sheepscott RR display in On30 and Mudhens display in HO3. Photo by J. Sauer.

the Twin Cities which was readily accepted. Here's the future convention line-up:

- 2023 — Denver, Colorado, August 30–September 3
- 2024 — Pittsburg, Pennsylvania
- 2025 — St. Louis, Missouri
- 2026 — Twin Cities, Minnesota
- 2027 — no bid

Linn Moedinger presented an informative “then and now” slide show on the East Broad Top Railroad, while talking about what has been accomplished by the new owners and their future plans.

Robin then made a few closing remarks, pronounced the convention closed, wished all safe travels and that all could meet again in Denver.

The 42<sup>nd</sup> NNGC certainly provided for meeting old friends, making new ones, seeing what is new, viewing inspiring models and layouts, learning from clinicians and just plain conversations with others. Truly the celebration of a fine fellowship.

If you are interested in a bit more on this convention, please see the NNGC YouTube channel at: Narrow Gauge Off the Beaten Track — YouTube and look for the 42<sup>nd</sup> National Narrow Gauge wrap up.

I want to thank Greg Wright and anyone else responsible for the surprise 90th birthday card signed by so many of you at the convention. I am honored and appreciate your well wishes.

Bob Brown

**Last Run...**

x Herb Koenig passed away July 26, 2022. He was 84. Herb helped organize the very first National Narrow Gauge Convention in 1981, and he played a major leadership role in the three subsequent St. Louis conventions. He was a member of the Friday Boomers, the Mudhens and a founding member of the St. Louis Area Narrow Gaugers (SLANG). Over the years he created a beautiful narrow gauge empire in his basement which he named the Cordite & Flatriver RR. It depicted the Colorado narrow gauge in all its glory. His mountain scenery was simply stunning. His HO/HOn3 layout was featured in the June 2001, Model Railroad Craftsman. Herb was a master craftsman and our go-to-guy for all things electronic. Our condolences to his wife, Reva, and to his son, Kris. Pete Smith.

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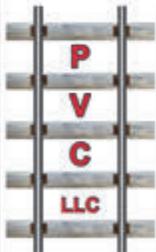
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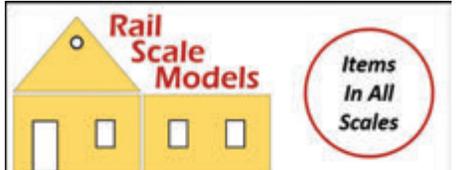


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# The Pigeon Hole

Dear Bob,

I built both cabooses by Banta (photo attached). They are nice models. The short caboose fits over the Bachmann flatcar minus the stake pockets. The other caboose fits over the stake pockets, making it wider! Not sure why but would have preferred the narrower version.

First class magazine!

Sincerely,

Jim Bullock  
Via email



Editor's Note: Letters chosen for publication in "The Pigeon Hole" may be edited for length and clarity.

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**WILD WEST**  
SCALE MODEL BUILDERS

Dear Bob,

I never imagined writing to you at the GAZETTE. When I first came across the magazine in the 1980s as a teenager, I was in awe of the standard of modelling. At the time I modelled in HO, then N, now On30. I picked up the March/April 2022 issue at the local model shop (Trainworld in Melbourne, Australia) last week and, surprise, saw the Craig Symington, MMR, article regarding a test track, which I was in the process of building a version myself. I acknowledge Craig's design, and as a late change adopted his idea of using a decoder in the unit to be able to drive analog DC locos.

Variations in my design are apparent from the photos, including:

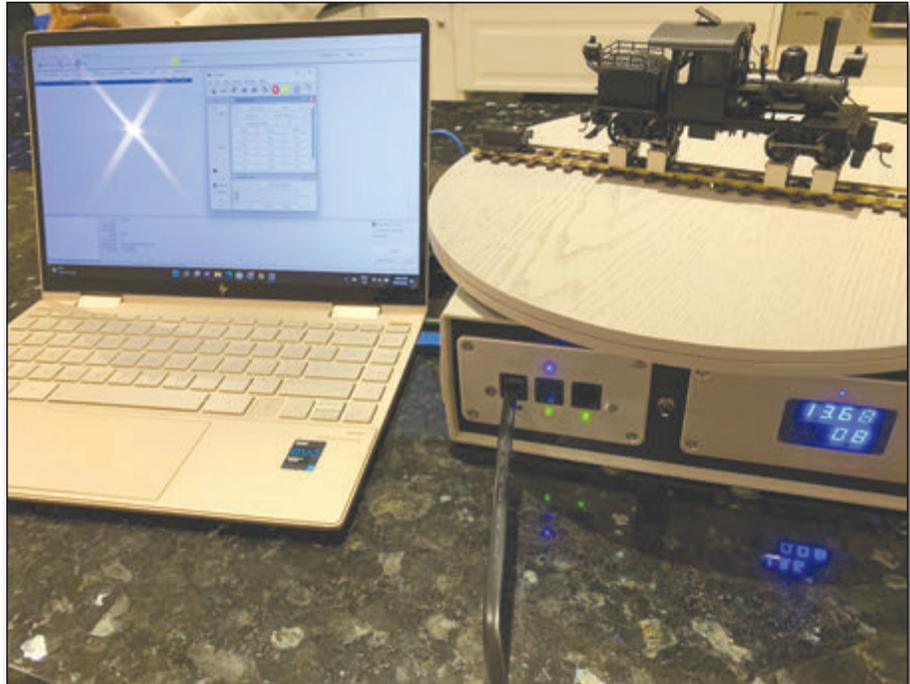
- DCC Concepts alpha panel
- DCC Concepts alpha meter
- Banana plug connections out to the test track
- Revolving test track on 'lazy susan' bearing with Bachmann E-Z Riders (rollers) and KD height gauges.
- USB interface in the rear panel.

It is great to be able to study a locomotive in motion but without it moving away and turn it from side to side to check things are operating symmetrically (if they should).

Keep up the great work!

Sincerely,

Rowan Cole  
Via email



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The HO diorama measures 38" x 22" and the standard gauge version is shown here.

Many more photos at [www.btsrr.com](http://www.btsrr.com)

Dear Bob,

In regards to your review of NO-OX in the July/August 2022 GAZETTE, car nuts like me are familiar with a similar product called di-electric grease. It's a white colored cream/paste which has an effect very similar to the Sanchem product. Available in multiple different sizes at any full line auto parts chain, like AutoZone.

Sincerely,

Jim McLaughlin  
Via email

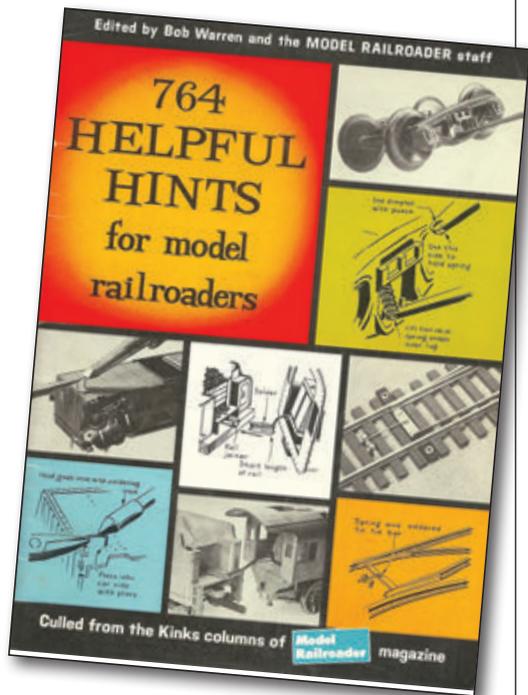
Dear Bob,

Here is a follow up to my letter in the last issue. I did the NO-OX last winter, so it has been on the track for eight or nine months without me doing anything despite the Roseville extreme hot and cold weather, my layout being in the garage and the fact that I don't run trains that frequently. No other track cleaner, and I have tried many, has ever lasted more than a week or two under those conditions. The NO-OX is also keeping good electrical contact with my HOn3 turnouts which only have manual throws and rely on the physical contact to make the electrical contact. No other cleaner has done that, either.

Keep up your great work on the GAZETTE!

Sincerely,

Bert Donlon  
Via email



Dear Bob,

After reading your "New in Review" in the July/August 2022 GAZETTE, I thought you'd be interested in this, regarding NO-OX. It's recommended in a book from 1965!

764 Helpful Hints for Model Railroaders — see tip #7-49, from the incomparable Linn Westcott.

Sincerely,

Paul Wilfong  
Via email

Dear Bob,

Here is my version of Jim Vail's little suspension bridge described in his column "Two Little Bridges" in the September/October 2011 GAZETTE. The filigreed angle braces are etched brass pieces I designed on my computer, following a photo of a real iron bridge. I had these made by PPD Limited (<https://ppd ltd.com>) as part of a larger custom order. The finials were 3D printed by Shapeways (<https://www.shapeways.com>) from a design I created in the free 3D design program, Blender. I cast the plaster abutments in a simple mold. Other than that, I followed Jim's instructions closely. I'm still planning my layout, in between making structures (my favorite sub-hobby), so the bridge has yet to find a home.

I really miss Jim's column. So many neat ideas!

Sincerely,

Sandy Stewart  
Via email



Dear Bob,

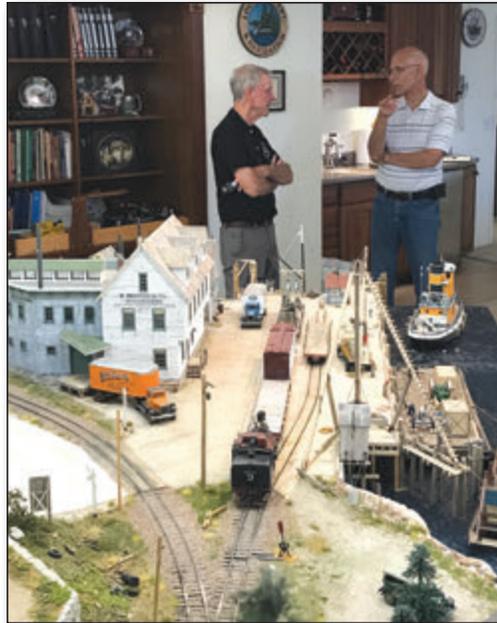
Recently on our way to St. Louis from Cincinnati, my wife, Nancy, and I had a chance to stop in Illinois to see Pete Smith's Loon Lake layout. Fortunately, I had read his recent GAZETTE article the week before, so was well-versed in his waterfront scene, and certainly enjoyed the logging, mining, and brewing portions of his layout, along with the bench work and fascia design, which I think was another GAZETTE article some time ago.

Pete had asked about obtaining drawings from my Reed Bros. Boatworks model article in the January/February 2017 GAZETTE, so maybe we'll see that in S scale at some point.

That's Pete on the left and me on the right.

Sincerely,

Sam Swanson  
Via email



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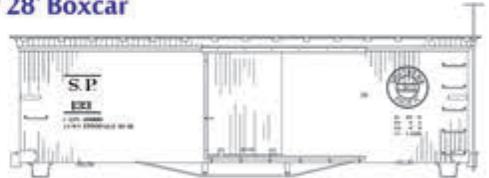
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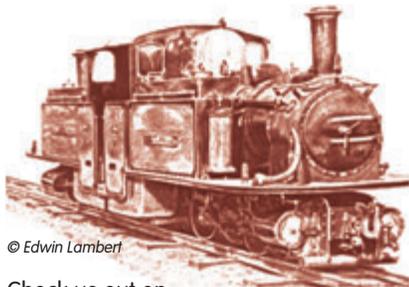


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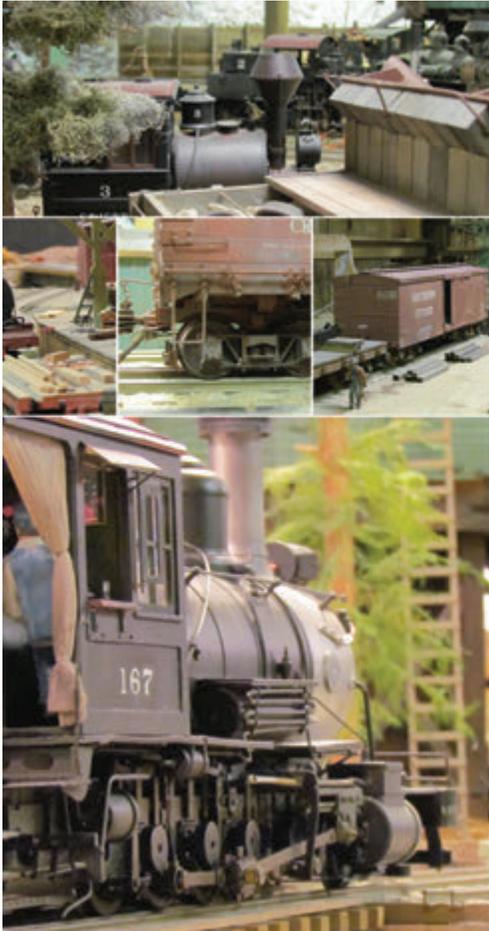
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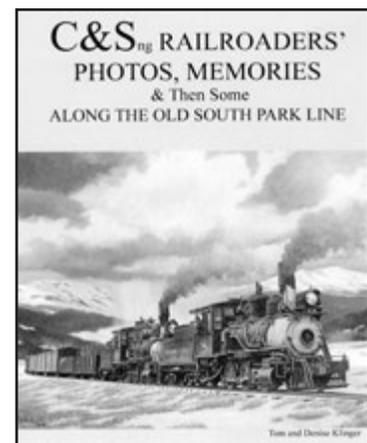
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# New in Review



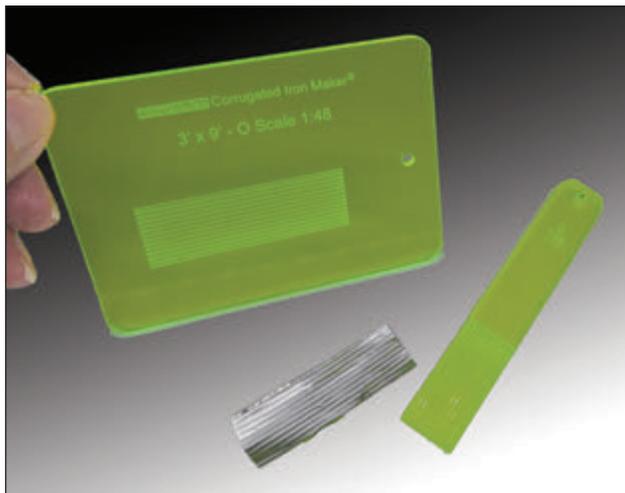
Leadville Designs, 5 Stapledon Cr., Ottawa ON, K2H 9L1, Canada, [www.leadvilledesigns.com](http://www.leadvilledesigns.com) sells kits for the 3000 series boxcar as originally built for the Denver & Rio Grande Railroad in 1903. To my knowledge, this version has not been offered before. Featuring a simpler air brake system, end truss rods and wood roof, the car also had less safety equipment than when rebuilt in the early 1920s when a metal roof, modern air brake system and added grabs were applied. I built the HOn3 version priced at \$44.95 and it is also offered in Sn3 at \$64.95 and On3 at \$84.95. All three less trucks and couplers. If you are not modeling the D&RG or modeling a later time frame, these cars also found their way into MOW service or could be lettered for another road. Since I model the 1949 era, I elected to letter the car for my own road but generally built it as designed. The kit consists of laser-cut wooden parts; 3D printed, etched brass and styrene parts, wire, nylon, and decals. There are two levels of detail possible with this kit. Every detail can be added or some details, later hidden, can be omitted. For the first time, the instructions even note this possibility and provide guidance on when to do so. As with earlier LD kits, the fit of the parts is superb and the assembly is precise. There are a few points of confusion. One example is the styrene custom cast corner angles, the middle ones of which are to support the end truss rods. The instructions refer to a diagram and a process of double-bending them to form the bracket for the truss rod. I could find no specific diagram other than the car plan, nor could I figure out the process. I elected just to apply them as an angle iron, omitting the end truss rods. I was very impressed by the delicate 3D cast piping attached to the air brake reservoir and the brake system can be added quite easily thanks to this.

No timing is suggested for adding couplers or decals

but it is easy to figure out the best stage to do so. I added Kadee #714 couplers, requiring a slight enlargement of the end sheathing and Grandt Line D&RGW 3-foot 7-inch trucks. Note that using the stock kit end buffer blocks and brass overlay plates will not allow proper operation of the Kadee couplers. If you wish, modification of the kit parts is easy to allow full coupler operation by removing the bottom half of each. I applied Hunterline Boxcar Brown stain (which is not brown) to the body and Floquil Red Oxide for the metal parts. This replicates the prototype's slight difference in color between wood and iron parts when painted and weathered. The underframe is detailed, adding those darned turnbuckles to the nylon truss rods is the most challenging step for me (at least in HOn3). The body is then added and the interior framing, present in other LD kits, is a nice feature. I stained mine a light brown wood color and will permanently park the car adjacent to a loading dock with an open door to show it being loaded. If made operable, be sure to add weight to bring it up to the NMRA recommended practice. A jig is provided to form grabs irons, but I elected to use commercial grabs. I also added four, not two, stirrup steps, and a few extra grabs. Note both the ends and roof have grab irons. All the door hardware is present and adds a lot to the final appearance. The roof and brake staff and wheel are added last. I used a combination of the kit decals and my own custom road name decals to letter the car. The provided decals are very fine quality and apply easily. They accept decal softening solutions well.

I was pleased with the result. This car joins other kits available for modelers to model the earlier period more easily in Colorado and California narrow gauge. I look forward to more releases from Leadville Designs. Charlie Getz.

Brunel Models, 20 Station Rd., Cheltenham, Victoria, 3192, Australia, [brunelmodels.net](http://brunelmodels.net) sells a few interesting tools. It would be worth your time to check out their web site. I be-



came aware of Brunel Models after reading Walker Embry's excellent article "Corrugated Steel from Tinfoil" in the July 2022 Railroad Model Craftsman. Walker used a tool called a Corrugated Iron Maker sold by Brunel Models and I just had to try one, so I placed an order with Brunel and soon had my Corrugated Iron Maker. This tool consists of a plastic plate with a scale 3- x 9-foot corrugated section in the middle. You place a piece of slightly over-size tin foil over the corrugations and run across it with a spatula-like tool making perfect corrugated ridges. Then you trim your corrugated piece to size. I suggest using small scissors.

The Corrugated Iron Makers come in HO scale, 1:35 scale, U.S. O (1:48) scale, European O (1:43) scale, N scale, 1:32 scale and 1:72 scale. My U.S. O scale tool cost \$17.50. I also ordered a \$3.00 package of tin foil so I would be sure of having the right thickness of foil. That made the bill \$20.50. I was surprised to see the postage was a whopping \$20.50, but what the heck, my tool and foil came by international air in only 10 days. Bob Brown.

Berkshire Valley Models, 438 Morgan Woods Dr., Fenton, MO, 63026, 314/401-4005. [www.berkshirevalleymodels.com](http://www.berkshirevalleymodels.com), latest O scale wagon kit sells for \$29.95 and assembles into a hook and ladder fire wagon. Wagons like these once hauled the ladders to fires along with long hooked poles used to pull apart burning material.

The instructions warn that this is a delicate kit and I agree. It takes some care to assemble. The front and rear spring assemblies consist of white metal castings that have to be ACC'd together. The front spring assembly fits on a cast white metal fifth wheel. The wheels are also white metal. The frame and superstructure are laser-cut wood and frail. You do have to bend six bucket hooks and four wires to hold running boards for the fireman to ride on. Templates are provided for bending these parts and holes are laser cut into the frame for these wires so no drilling is necessary. I assembled my spring assemblies and sprayed them, then sprayed the body and the wheels. Once I had the wheels on the axles, I added the spring assemblies to the frame.

I did have trouble with the six water buckets. The instruc-

tions say to drill two holes in dimples in the upper edges of each bucket. Then use the template provided to bend wire handles for the bucket handles. This was beyond my skills, so I bent a U-shaped wire for each bucket, filled a bucket with ACC and stuck the wire handle down into the ACC. Crude, but it seemed to work.

The ladders are laser-cut wood including the rungs. A neat jig is provided for either 24- or 21-inch wide ladders and they assembled easily.

White metal hooks are provided to ACC to the ends of wires to make the hooks in the "Hook and Ladder Wagon." The instructions were clear, and I used them and the photos to assemble my wagon.

I am pleased with my hook and ladder wagon and hope Berkshire brings out kits for more horse-drawn fire equipment. Even if you are more modern, one of these wagons might be in back of your fire house for use in a parade. Bob Brown.



Wild West Scale Model Builders, P.O. Box 1971, Englewood, Colorado 80150, 303/842-6106, sales@wildwestmodels.com, www.wildwestscalemodels.com sells corrugated paper roofing. Way back in the late 50s Cliff Grandt used to hold a gathering of narrow gauge modelers every fourth Friday of the month. Cliff had a little corrugator that would make paper corrugated material. One Friday, I came equipped with strips of paper and spent the whole evening corrugating a lifetime supply of material. Well, not really, it soon ran out and I began using the corrugated paper sold by Russ Simpson. So, I love paper corrugated material. It is easy to cut, glue down, and paint will not flake

off it. As my supply began to run out, I decided to try some of Wild West's paper corrugated roofing. It comes in N, HO, S, and O scale. I bought some O scale material. Sixteen 11-inch long, 27-scale-inch wide strips sell for \$10.00. The paper is a nice aluminum color, and one package covers some 30 scale feet. The strips come in a plastic sleeve with coloring and weathering instructions. I found the material nice and stiff so glue trails will not show through it. I am looking forward to using this corrugated on my next Bodie model.

I also ordered a roll of double face tape sold by Wild West. It is designed to hold down Wild West's shakes and corrugated roofing and sells for \$12.00 for a 1/2-inch wide by 180-foot long roll. I find it perfect for holding down plastic



windows and doors while spraying them. For years I have used masking tape for this. I would cut a strip of tape, lay it on a piece of cardboard sticky side up and tape each end with small strips of masking tape. It would stick to me and was sort of like corralling monkeys. So, I decided to try the Wild West tape. Now I just cut a piece of tape, lay it down (no monkeys) peel off the backing and have a nice thin, almost invisible, strip of adhesive. I stick my parts down and spray. Of course, you can also use this tape to hold down your shakes and corrugated roofing. Bob Brown.



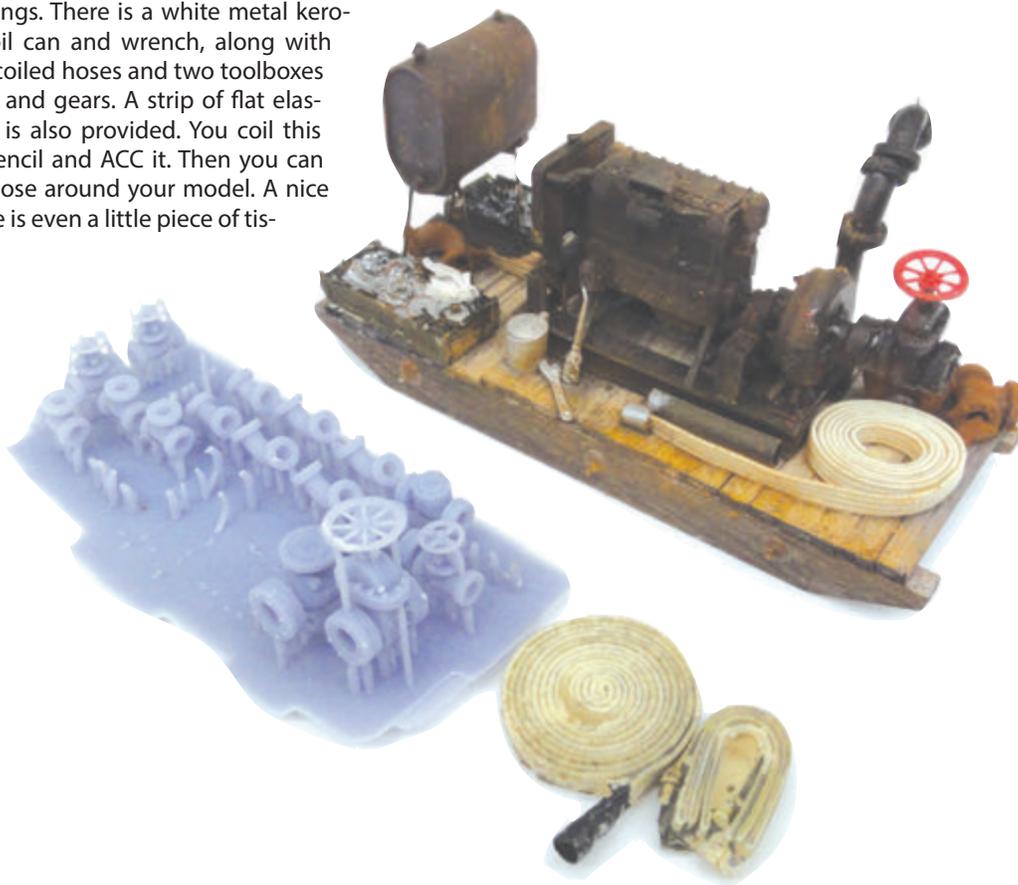
Conowingo Models, 440 McCauley Rd., Conowingo, MD 21918, Conowingomodels@yahoo.com, <http://conowingomodels.com> sells HO scale kits for a 24-foot wood tank water car. The kits sell for \$40.00 each less trucks and couplers. They feature wood construction with Tichy details, 3D custom parts, wire truss rods and a set of Conowingo Railroad decals. Trucks and couplers can be purchased from Conowingo Models. Bob Brown.

Hangman Creek Diorama Company, 315 W. 30<sup>th</sup> St., Sioux Falls, SD 57105, 650/679-5854, jessd3818@yahoo.com sells a series of O scale kits designed to assemble into detailed dioramas. Examples include pond boats for pushing logs around log ponds, a pipe rack, small trestle, blacksmith car, bridgetender's shack/tool shed, twin water tanks based on the Yosemite Valley's Bagby tank, and the kit I chose to order, a portable Diesel-powered centrifugal pump sled. My sled kit sells for \$65.00 plus \$20.00 postage.

The Diesel engine, radiator, and pump come mounted on an angle iron frame as a one-piece 3D print. I was astounded by this print, all I had to do is add ignition wires, a pipe and paint and weather the print. The fuel tank is also 3D and I had to bend up two legs from wire. The wood sled must be cut to length and a strip wood deck added along with tie rods and N-B-W castings. There is a white metal kerosene can, oil can and wrench, along with two epoxy coiled hoses and two toolboxes full of tools and gears. A strip of flat elastic material is also provided. You coil this around a pencil and ACC it. Then you can drape the hose around your model. A nice touch. There is even a little piece of tis-

sue paper for oily rags. The kit also includes some extra strip wood to strew around and some styrene tube for pipes. A sheet of 3D pipe elbows and valves is also provided. I had 14 of these plumbing parts left over after detailing my pump and can use them for future projects. The idea is to assemble the pump and use it in a detailed diorama, so it has lots of details.

The instructions come in a 27-page, color, horizontal format, 8½- x 11-inch, spiral bound booklet. This booklet discusses inspiration, modeling philosophy, what's in the box, supplies and tools needed, preparing the various parts, and assembling the pump sled and painting and weathering its parts. The instructions are really for the novice, but there are some gems of wisdom to be found. I enjoyed assembling my portable pump and it makes a nice piece of clutter on my already cluttered layout. Bob Brown.



Beware: I reviewed an electrical enhancing material called NO-OX in the July/August issue. After applying it, I noticed my layout began shorting. I thought it might be a closed gap, so I ran a razor blade through all my gaps and my layout stopped shorting. Then I realized that the NO-OX, due to its Vaseline like consistency, may have shorted across one of my gaps. So be careful when rubbing NO-OX on and off your track. Bob Brown.

Eric Bracher has sold Rio Grande Models to Craig Tribuzi. The kits that Rio Grande Models produced are starting to become available on the new [www.RioGrandeModels.com](http://www.RioGrandeModels.com) website as soon as they can be produced. The sale includes the rubber molds, casting equipment and supplies to make the fine scale kits that Rio Grande Models is known for. Eric continues to remain active as a consultant to ensure that the quality of the kits he produced continues. Craig has also completed the Simpson Models website. Go to [www.simpsonmodels.com](http://www.simpsonmodels.com) for more details. Bob Brown.

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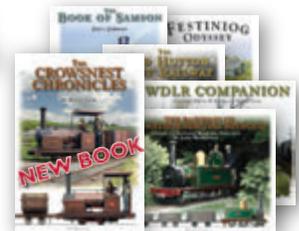
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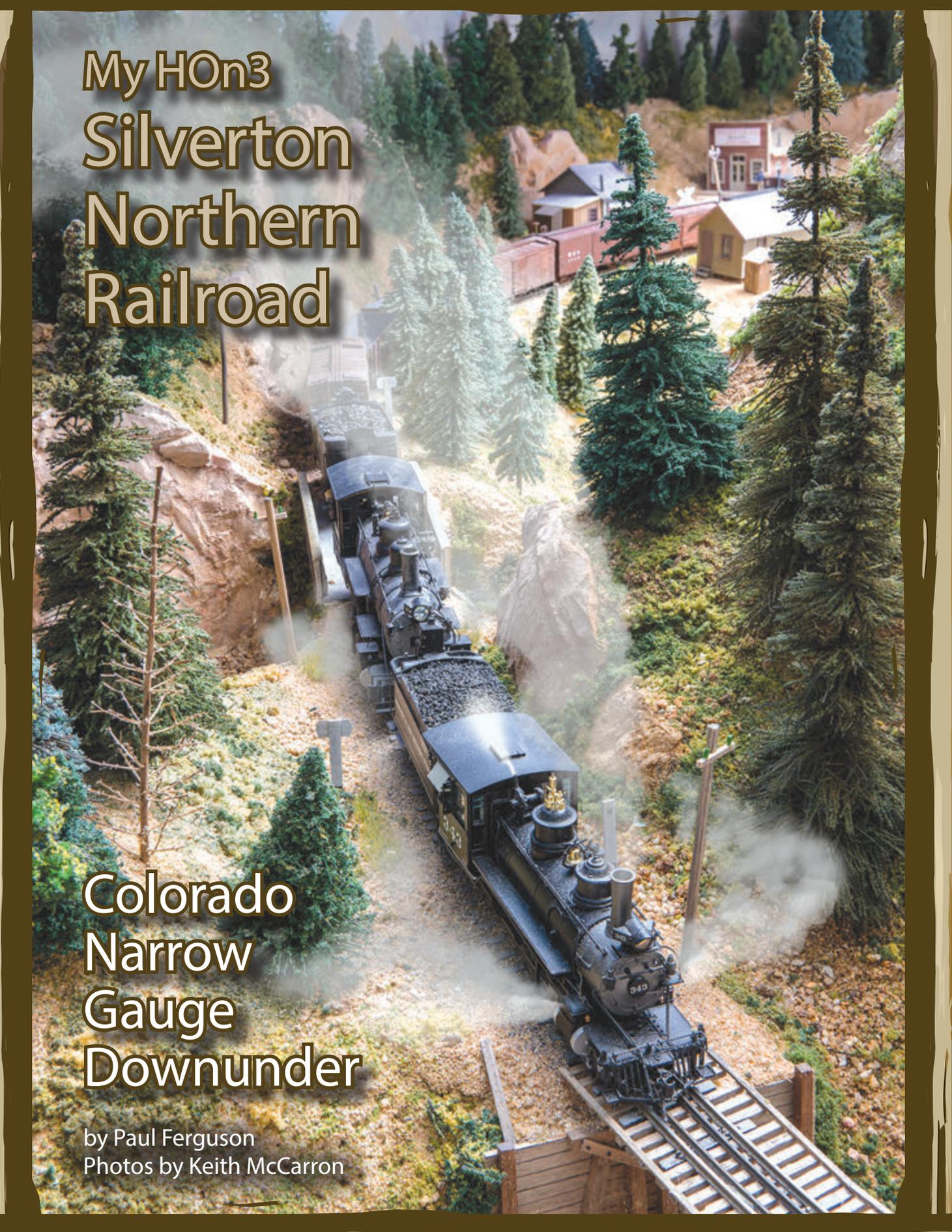
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My HO<sub>n</sub>3  
Silverton  
Northern  
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Colorado  
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Downunder

by Paul Ferguson  
Photos by Keith McCarron

## Background p

While my HO<sub>3</sub> Silverton Northern Railroad (SN) has already celebrated its 25th Anniversary, I really started modelling narrow gauge in the early 1980s. I was happy building an HO short line layout until I attended a presentation in Sydney by Malcolm Furlow on Colorado narrow gauge railroads and his Rio Chama RR. I became hooked and immediately purchased an HO<sub>3</sub> locomotive and some freight car kits.

I had no prototype in mind at first, but an article about the extension of the Silverton Railroad to Ouray, and then the purchase of a copy of *The Rainbow Route* resulted in a layout modelling Silverton, the Silverton RR and the Silverton Northern. Shortly after, I also became a member

of the San Juan County Historical Society, which has proved a great decision as the folks there have provided so much great information and assistance over the years.

My first layout lasted about 10 years. The second, started in 1994, was built in a 20- x 12-foot shed on two levels joined by a 2½ turn helix. It had a generic Silverton, Burro Bridge, Red Mountain, and Ouray on the lower level, while the SN was the upper level with Howardsville, Eureka and Animas Forks. The helix also hid a 3-track storage yard for the lower level which represented either Durango or Ridgway.

The layout plans changed in 2004 when I attended the Santa Clara National Narrow Gauge Convention and then visited Silverton. I was just so taken by the scenery, the ambience and character of Silverton that I decided then and there that I had to model Silverton as it looked in 1941.

As soon as I got home, the helix and Ouray sections were removed, the Silverton area was enlarged, and track was re-laid so when I was finished I had what is the current SN. It still had the same two levels and basic configuration as the previous layout, except the two levels were now connected by a 4 percent grade commencing at Ironton.

This re-build led to three major outcomes. Firstly, I got to research and scratchbuild all those buildings in Silverton, and secondly, the scened areas on the layout were greatly increased.

However, in my excitement to complete these changes, I forgot about the replacement hidden staging area, and unfortunately my bench work construction methods meant I couldn't include one without another major re-build. That moment of

(text continued on page 26)

Title photo opposite:  
Double header crossing  
Rio Animas after leaving  
Ironton.

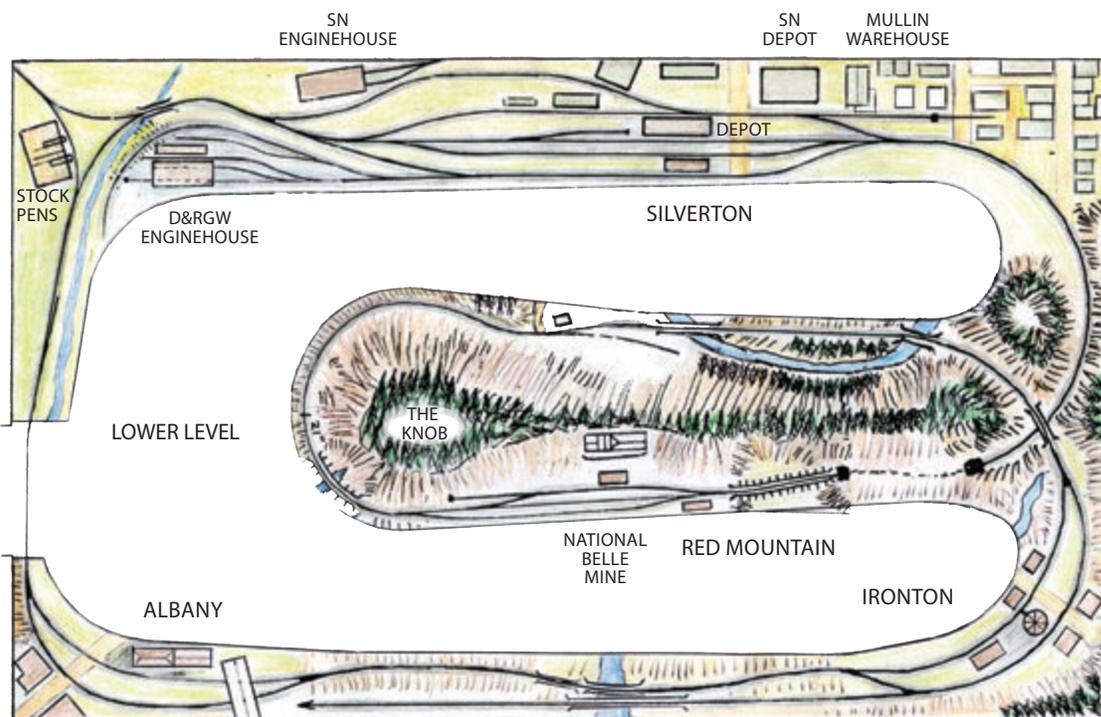
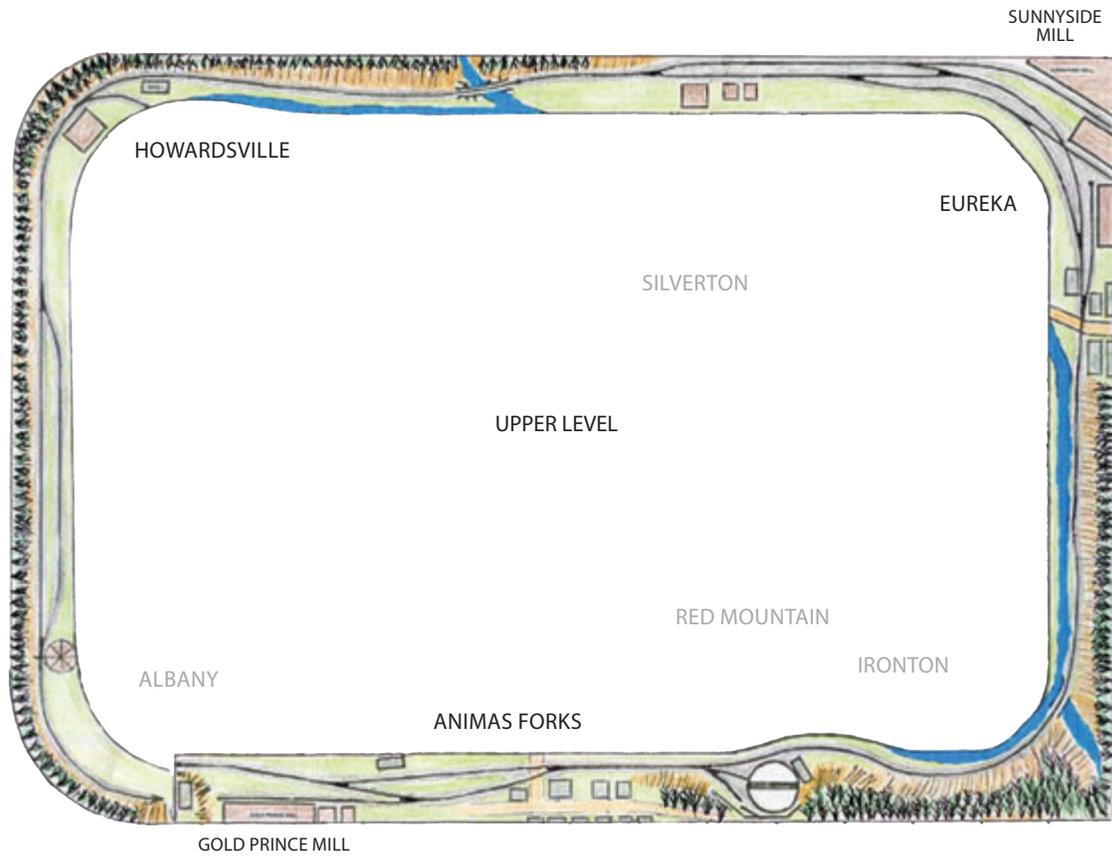
Right: Number 278  
switching Eureka Yard.

Below: Number 278  
switches concentrate  
loaded boxcars at the  
Sunnyside Mill in Eureka.



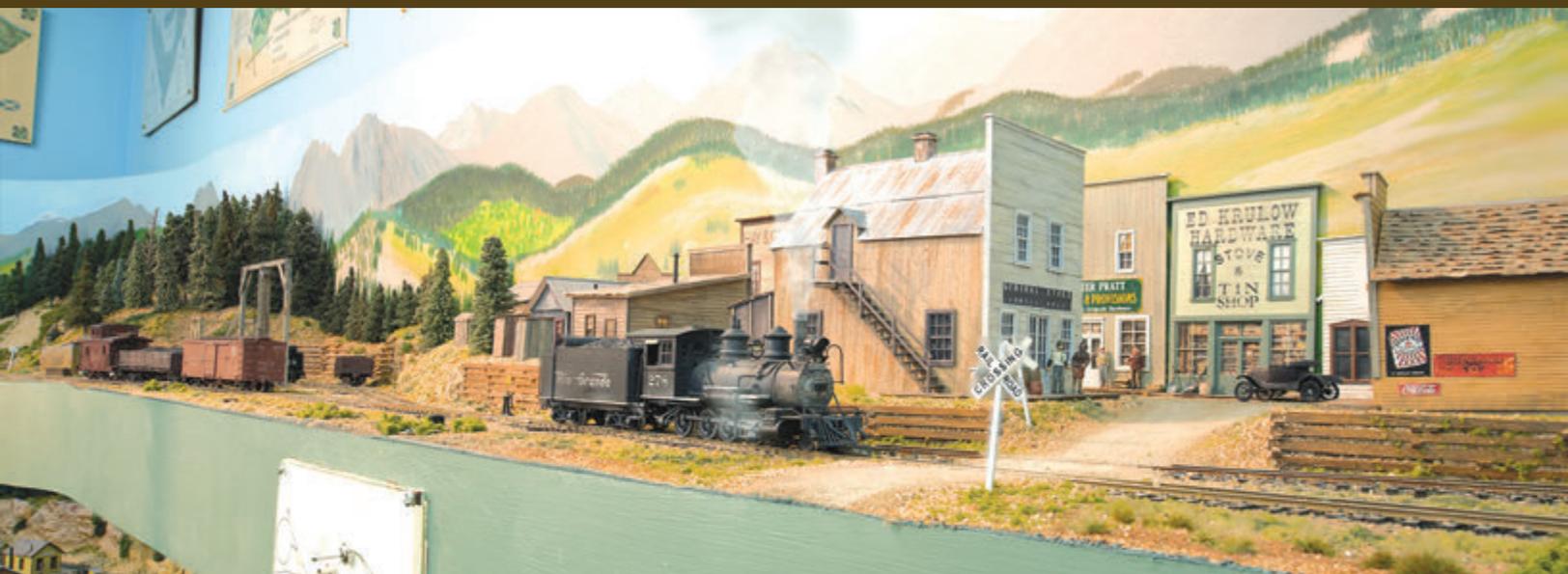
# THE HO<sub>n</sub>3 SILVERTON NORTHERN

DRAWN BY THE AUTHOR  
NOT TO SCALE

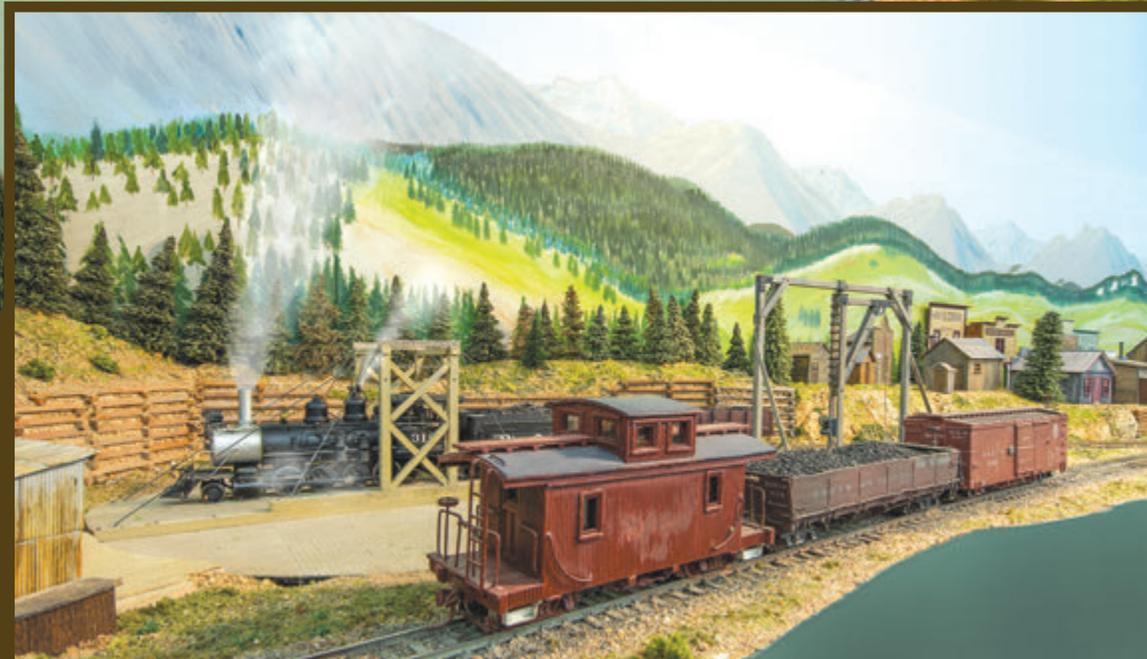




Above: Overall view of the Silverton Northern with the builder/author.



Above: Number 278 simmers at Animas Forks while the crew takes a break.



Right: Number 318 being turned before switching the Gold Prince Mill in Animas Forks.



Above: The Gold Prince Mill at Animas Forks.



Left: Number 318 will switch the Gold Prince Mill in Animas Forks after being turned.

(text continued from page 23)

forgetfulness still hasn't been resolved!

Since my SN, while set in 1941, does not strictly follow the prototype, I feel it can best be described as freelanced prototype. My philosophy is that while some scenes may not be quite correct for 1941, they are generally based on a prototype from an earlier period or come from another loca-

tion in the Silverton area.

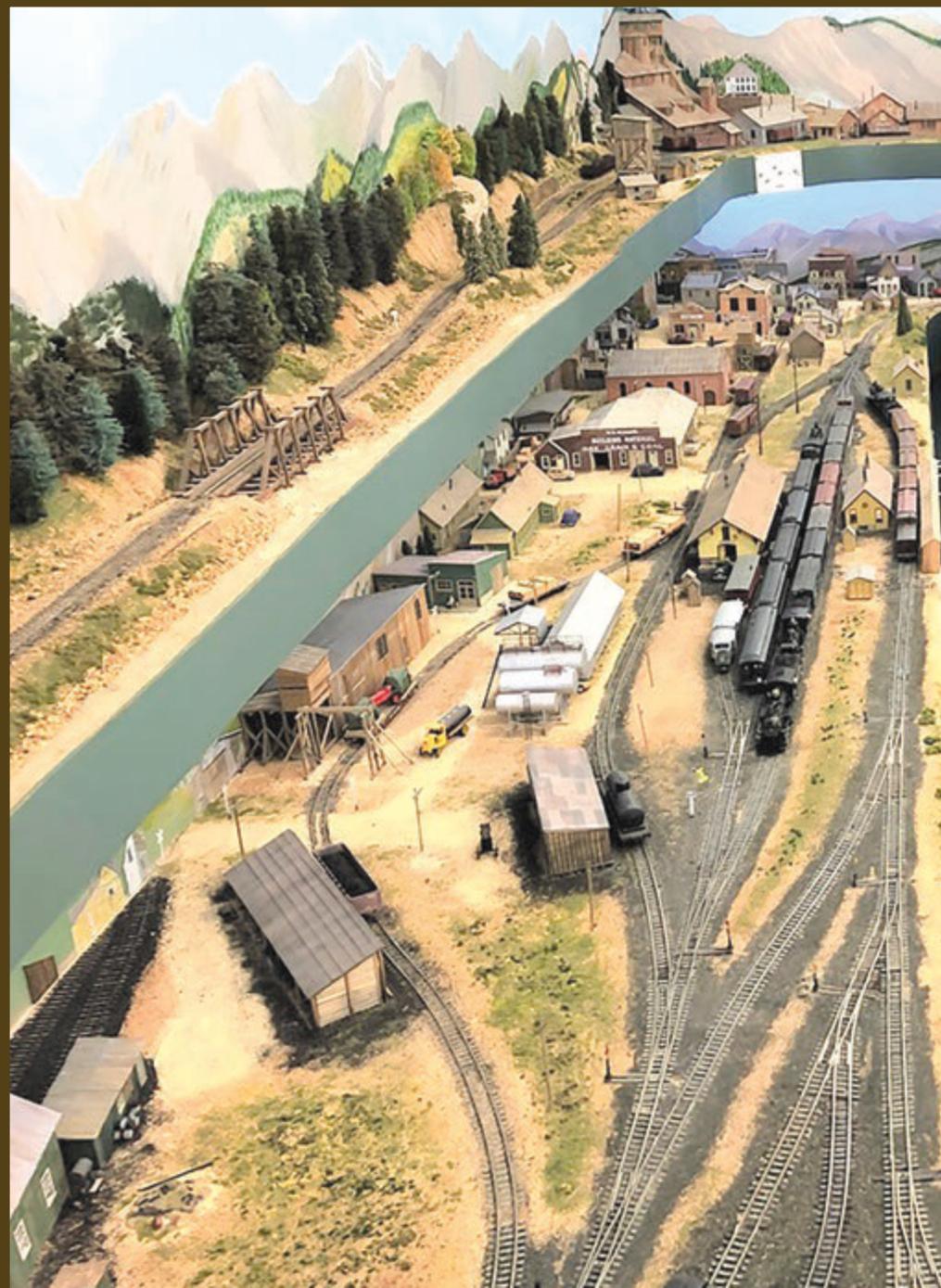
The main example of this philosophy is that since the SN tracks were in place and scrap trains ran in 1942, the SN could have been operating in 1941 if history had been different.

Other examples of this philosophy are that the Sunnyside Mill in Eureka is the pre-1917 wooden mill, the tracks to Animas

Forks are still in use, and the towns of Eureka, Animas Forks, Red Mountain, Ironton, and Albany are bustling little communities and definitely not the near ghost towns they were in 1941. Furthermore, visitors notice that Albany looks a lot like Ophir, on the Rio Grande Southern RR, that the Shavano still runs from Salida to Durango through Silverton, and that my Ironton is



Above: The Shavano arriving in Silverton.



Left: Overview of Silverton with Eureka above.

the fictional junction for the D&RGW and Silverton Northern.

## Layout Construction **p**

The SN is basic and is operated by two DC hand-held throttles with five plug in points, so I can follow trains around the layout on both levels. The track is Shinohara code 70 flex track with #4 and #6 turnouts. Slide switches located on the edge of the layout are used to manually throw the turnouts and to route power.

In building a double deck layout, I decided to keep the upper deck as narrow as possible, both to eliminate shadows and to avoid compromising the lower-level scenes. As a result, the top shelf varies between 8- and 12-inches, with Eureka being modelled in a corner to offer maximum space for track and structures. The SN also features prototypical 2, 3 and 4 percent grades resulting from elevation changes included in the design, with Albany at 43-inches the lowest point, and Animas Forks at 60-inches the highest point of the layout. Lighting is provided by fluorescent tubes and LED strips mounted under the upper deck.

Scenery on the SN is a mix of hard-shell, plaster impregnated gauze on cardboard lattice, and carved sheet Styrofoam. Plaster rock castings, shale pieces, crushed sandstone, decomposed granite, kitty litter and ground foam from many different manufac-

turers cover those base surfaces.

There are 1500 trees on the layout, mainly the pines that form scene blocks or that run along the backdrops to give the illusion of more depth to the narrow shelves. Several Aspen groves have been included to establish the early Fall setting. The backdrops are based largely on photos of the area and given my basic artistic skills, I am reasonably pleased with the outcomes.

There are more than 150 structures on the layout, and of those 100 have been scratchbuilt. Most scratchbuilt structures are layout models built from photos, drawings or plans to capture the look of the prototype, but in many cases modified to fit the available space.

Most of the kit-built structures are in the downtown area of Silverton and were used because they have a resemblance to buildings in Silverton. Most of my structures are built on a styrene sheet base, both for ease of adding scenery and for ease of removal, if I decide to change a scene. One strong influence on the structures of the SN has been Anvil Mountain Models. Lowell Ross, an SN modeler, who owned the company, very generously shared some of his research with me, and that led to upgrades to several of my models and so there are 12 structures on the SN that have been either built from, or inspired by, AMM kits.

The SN has a stable of D&RGW locomotives — a K-28, three K-27s, a C-25, three C-19s, a C-18 and two C-16s, all of which, except one (K-27 #460), operated on the Silverton branch in the 1930–40s period. The rolling stock roster is around 100 cars, again mostly D&RGW but including about a dozen SN and RGS cars. The rolling stock is largely kit or scratchbuilt, with just 10 Blackstone Models cars in the roster.



Above: Silverton enginehouse area with numbers 278 and 268 switching.

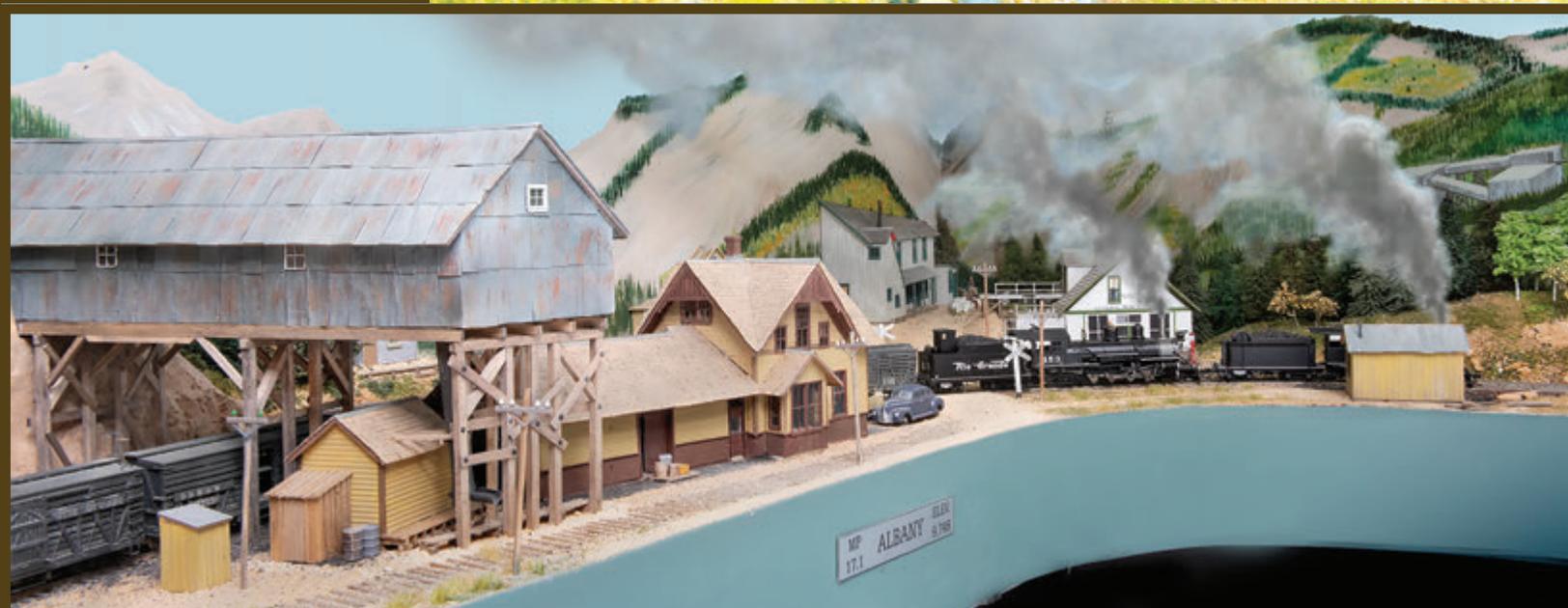


Left: Number 375 Leaves Silverton engine servicing area.

Right: Extra 343 heading to Albany while Silverton Northern passenger train from Albany crosses the trestle on the author's HO3 Silverton Northern.



Below: Stock train leaving Albany ready to attack the grade ahead. Note how the author's scene resembles Ophir.



Whilst trains on the SN have a prototype look, the reality is that in the era I model, the D&RGW Silverton mixed only ran once or twice a week and the SN only ran as required.

With this limited prototype operation and given there is no hidden staging that can be used as an off the layout destination, I don't really have much scope for scheduled operations, and in fact, I have never operated my SN, as such.

## Future Plans **p**

My philosophy in building the SN has always been to give a scene a basic layer of structures and scenery and move on, com-

ing back to change or enhance scenes bit by bit.

So, while the SN is nearing "completion," there is always more internet information becoming available and so plenty of project ideas to refresh the SN.

Future projects are replacing generic buildings in Silverton with Colorado prototypes, re-building part of the Red Mountain scene with structures from Hermosa and replacing the generic enginehouse in Silverton with the 2 stall D&RGW roundhouse that existed until 1937.

It goes without saying that I will eventually find a way to include a staging yard without needing another rebuild — that would be the icing on the cake!

So, after a journey of 35 years, I am more

than happy with my efforts, and know I still have years of enjoyment ahead. Sure, while I wish I had done some things differently, the SN gives me a great deal of pleasure just watching trains run through the San Juan Mountains into Silverton.

Finally, I'd like to thank Keith McCarron for his photography skills that have so enhanced the look of the SN. x

# OUR WHITE PASS & YUKON ROUTE #81s IN HOn3



## A TALE OF COLLABORATION

by Mike May with Robert Bell

Photos as noted

### AN INTERESTING IDEA

Like most modeling projects, this one started with a great idea, albeit a little crazy seeming. The two of us have been friends and chatting all-things White Pass and modeling for some time now, always brainstorming projects and solving modeling challenges. The White Pass is one of those roads that isn't the easiest to model due to the lack of available models and parts. One Diesel locomotive prototype that we've talked about is #81 which was likely better known as U.S. Army #3000. As far as we knew it would need to be a complete scratchbuild project, at least until one slightly crazy idea was floated. This is the tale of two locomotive models, built on the same premise at the same time, but on different sides of the country and with different stories.

Both Rob and I model the White Pass & Yukon Route in HOn3. My (Mike) layout has graced the pages of the HOn3 Annual, the GAZETTE, and Great Model Railroads 2021. Rob's layout is set in 1983, a year after the prototype shut down. His layout is only a few years old and still under construction, but is set to represent the White Pass circa 1955 to 1975. Both of our layouts have tak-

en modeler's license on different aspects of the prototype which relate to the different takes on this locomotive.

### LOCOMOTIVE #81'S HISTORY

In the mid 1950s, the United States Army approached several locomotive builders for experimental Diesels for possible foreign service. One such result was the U.S. Army Transportation Corps #3000 built by General Electric in 1957 and powered by an Alco 251 I6 Diesel. This locomotive had the ability to operate on rail gauges of 42-inches, 39-inches (meter), or 36-inches by moving the wheels on one side in or out as needed. The locomotive was centered when configured for meter gauge track, but was slightly off balance when gauged for 36- or 42-inch gauge. Not necessarily an attractive locomotive, it could best be described as unique.

Specifications for this locomotive included several unique features for foreign service such as it's low height and narrow width, with a relatively high clearance over the track using 40-inch diameter wheels. The unit was built for extreme cold weather conditions and could be started without external help at temperatures

down to -50 degrees Fahrenheit due to special low temperature batteries. Other unique features included double-paned, insulated heated windows, and blind center wheels on the six-wheel trucks.

Number 3000 had slightly more tractive effort than a D&RGW K-37, at 38,000 lbs., and the locomotive was tested on the Denver & Rio Grande Western in Durango, Colorado, until 1960. It was then stored until 1973 when the Army sold it back to G.E., who in turn sold it to the White Pass & Yukon Route in southeast Alaska where it became their #81 in the fall of 1973; the second locomotive to carry this number on the White Pass. The White Pass was a very busy, modern narrow gauge railroad at the time, and thus they needed a switcher for the Skagway end of the railroad. They had started to dieselize in 1954, and this slightly used engine must have seemed like a good deal.

The White Pass crews like the earlier D&RGW crews, complained about the visibility over the long hood of the locomotive and the cramped cab. So the shop installed bay windows, and later they raised the roof making the Diesel even odder looking. Wider walkways and full length handrails were also added. According to John Stutz, #81 was in a dead line behind the shops by October 1978, and according

to Bryan Saul it was shipped to Bandegua (a Guatemalan subsidiary of Del Monte Foods) in 1979 where it became their #314. Reportedly it was scrapped by 2006.

## THE AUSTRALIAN CONNECTION

As such a unique prototype, no models of this locomotive have been produced commercially. On an unrelated internet

Title photo opposite:

Rob Bell's White Pass & Yukon Route #81 drags a ballast train upgrade at Clifton, Alaska, on his layout. The locomotive was formerly, and likely, better known as United States Army Number 3000, an experimental export Diesel locomotive that was tested extensively in Durango, Colorado, on the D&RGW. Photo by Rob Bell.

Below: The prototype White Pass & Yukon Route #81 sitting near the Skagway shops in the summer of 1977. Photo by Bryan Saul, Rob Bell collection.

search one day, Rob ran across HO scale models of another unique narrow gauge Diesel locomotive, Australia's Queensland Railway class 1720. The similarities to #81 were immediately apparent. The Queensland Railway is a 42-inch narrow gauge railroad that had 56 class 1720 locomotives built between 1966 and 1970, powered by EMD 645s. The model of this prototype is actually an HOn3.5 model made to run on 12mm gauge track by the Australian manufacturer Wuiske (pronounced Whiskey) Models. Their website is [www.wuiskemodels.com](http://www.wuiskemodels.com) and they have a Facebook page as well.

With an idea to convert one of these models into a recognizable model of the 81, more research and general proportion estimates began. Information is scarce for the 81, the main reference material being an article by Ed Cass in the magazine Short and Narrow Rails issue 48. Additional photographs were also provided by Bryan Saul, John Stutz, and a few others.

Rob purchased his 1720 class model on eBay and ordered it, but they can be ordered directly from Wuiske, where I acquired mine. These locomotives retail for \$295.00 AUD (Australian Dollars), which at

the time came out to about \$200.00 USD plus shipping.

So how does a 1720 compare to the real 81? Well, #81 is 38-feet 1-inch long compared to 44-feet for the 1720. The model's 37-inch diameter wheels and unequal axle spacing on the trucks were close. Cab and hood heights also compared favorably.

## REGAUGING THE AUSSIE MODEL

The Wuiske 1720 class is very easy to regauge to HOn3. To do so, first remove the gearbox cover plate from each truck with the six clips molded onto the plate. Cut off the lip on the outside of the plate with a sharp blade, leaving the side clips to reinstall the cover plate. Using a razor saw to cut vertically next to these clips makes quick work of this. Then remove the wheelsets and press each wheel further in on its axle. Using a piece of steel and a small ball peen hammer, the wheels can be tapped in on the axles, but be careful that the wheel only moves about 0.030-inch in and stays true to the axle. Check the gauge with an NMRA standards gauge. Reinstall wheelsets, snap the cover plates back on





and test run. On Rob's, the wheels were pushed slightly more to one side to replicate the slight "off-balance" look of the prototype.

### DECODER AND SOUND

We both used a SoundTraxx Tsunami II Alco sound decoder, using the 251I6 prime mover option (Alco 251c.i.d., Inline 6-cylinder). The models are DCC ready with an NMRA 8-pin plug and provisions for speaker mounting. There is even a groove for the speaker wires to be routed on the chassis. Both of us used a modified iPhone 5 speaker hidden in the fuel tank, and TCS KA-4 Keep Alives. Each of our models used different mounting choices for the

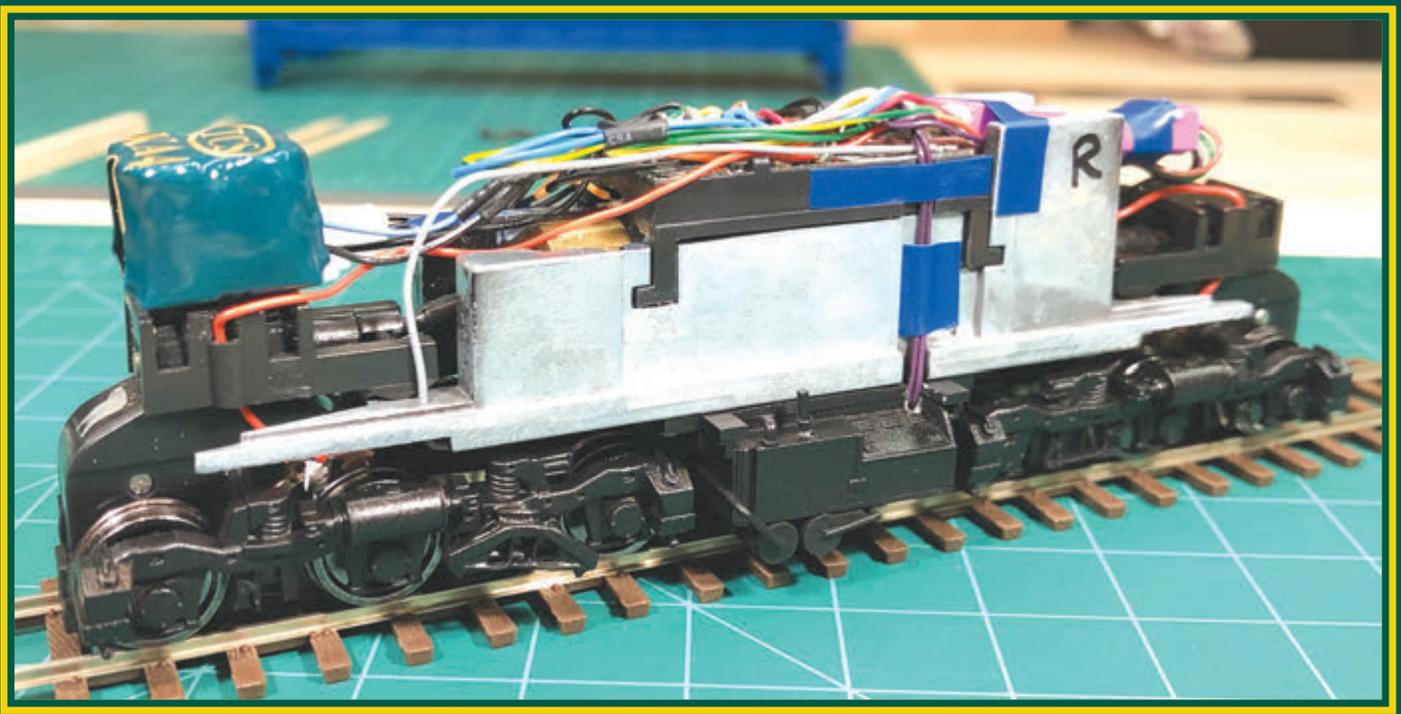
DCC equipment though. Rob mounted his Keep Alive to the underside of the long hood directly behind the light board using Loctite's Go 2 Glue. This puts the Keep Alive above the truck gearing and out of the way. I mounted my Keep Alive to the top of the front truck gearbox with epoxy. Both models had their metal frames milled out slightly to hold the decoder above this same truck. Keep in mind that there is surprisingly little room under the body shell for any extras.

### BODY MODIFICATIONS

We took different approaches to the body modifications due to our differing layout timeframes and individual model-

Above: The stock Wuiske Models 1720 Class locomotive is a sharp looking RTR model and the comparisons to #81 are very apparent. Photo by Mike May.

Below: This is the completed chassis of Mike's model with the DCC components installed. The Keep Alive is mounted on top of the gearbox so that when the shell of the locomotive is removed, no wires are left connected between the shell and the frame. Photo by Mike May.



ing license. If you are not modeling the White Pass, you have even more freedom to make more or less modifications.

Both of us started by removing most of the separately applied details and removing the paint. Do not try paint removers as none that were attempted touched the Wuiske paint without leaving the body immersed far beyond the recommended time for plastic. Instead, use an air eraser with aluminum oxide to strip the paint, you will be happier — trust us.

Once the body was lacking paint and details, the fun began. Both of us added styrene to “beef up” the frame. I used .125-inch C-channels and Rob used 0.080- x 0.188-inch styrene strips. The fuel tank was also shortened in height and a pair of main reservoir tanks were added below it. I came up with the idea of using a dental scraper to remove the ridges from the sides of the Aussie cab, and Rob took this technique to the extreme and removed every ridge from the cab and the body. He also used this same tool to remove all of the mold-on detail from the end beams.

## THE LONG HOOD

I cut out some of the side doors from the front of the long hood and

Below: Here the long hood and cab of Mike’s model are completed and ready for paint. Photo by Mike May.



Above: The fuel tank on the 1720 class locomotive was too large as compared to the #81. Mike shortened its height and built a new bottom from styrene. Main reservoir air tanks were also added below it like the prototype. Photo by Mike May.



Above: Rob’s detailed end beam showing the scratchbuilt polling pockets with N-B-W castings, lift rings, uncoupling lever, hand grab irons, M.U. connection, foot boards with brass bolts, and a brass air hose. Photo by Rob Bell.

replaced them with radiator grills, and used Tichy plastic handrails for the long handrails that are not on the Aussie model. Rob also used the Tichy handrails, but he cut off the plastic handrails and carefully drilled each with a #80 drill and used 0.010-inch music wire for the handrails. Rob used sheet styrene to fill in the air inlet openings on both sides, and made new doors for another section closer to the cab where the ribs had been removed. Bits of 0.020-inch styrene rod were used to simulate the hinges on these new doors. The latches were carefully sliced off of the doors at the very front and then glued in place on these new doors. Both sides of the front were filled and/or sanded as needed for the new radiator shutters and framing closer to the prototype 0.020-inches. V-groove styrene with 0.050-inch spacing was used to simulate the radiator shutters, and 1/32-inch brass angle was used to make the frame.

## ROB’S COUPLER MOUNTS

The prototype could have the couplers mounted at several heights from 22½ up to 36-inches, and the model needed a better way to simulate the space above and below the prototype’s coupler. Rob came up with a method that effectively simulates this. First the coupler mounting screw holes were drilled out on the loco-



tive frame with a #46 (0.080-inch) drill bit. Next, Rob bored out a piece of 0.125-inch (1/8") styrene tube to a depth of about .25-inch with the same #46 (0.080-inch) drill bit and then cut the tube into two lengths of about 0.090-inch each. Then cut two approximately .25-inch lengths of 0.080-inch diameter styrene rod, and glued one of these into each tube; making a pair of what looked like "Battleship" game pegs. A #56 hole was carefully drilled in the center of the large end of each "peg" for a Kadee 714 mounting screw, or you could drill and tap for an 0-80 machine screw. The small end needs to be carefully sanded or filed just a little so that it fits snugly but is removable because the height of the big end may need to be adjusted. With the locomotive reassembled and on the track, the coupler height can be checked and adjusted as needed by trimming the large end of the peg. This took three or four tries and only ended up messing up one peg that required starting over! The Kadee 714 HOn3 couplers were then installed, but still needed to have the rear portion of their boxes trimmed off to clear the locomotive's trucks.

## CAB CHANGES

To replicate the distinctive raised cab roof, I cut a rectangular hole in the existing cab roof and the addition was built with sheet styrene cemented together. The seams were sanded to a slight radius to simulate the welded construction. Rob made his in the same way, but did not cut a hole in the roof, instead opting to sand the lower portion of the extension to the cab roof radii and cemented it in place. Rob also meticulously planned and milled out new lower windows on the rear of the cab. New window edges were formed from Tichy phosphor-bronze wire, bent and glued to edges of these new window openings with CA.

## LIGHTING BOARD UPDATES

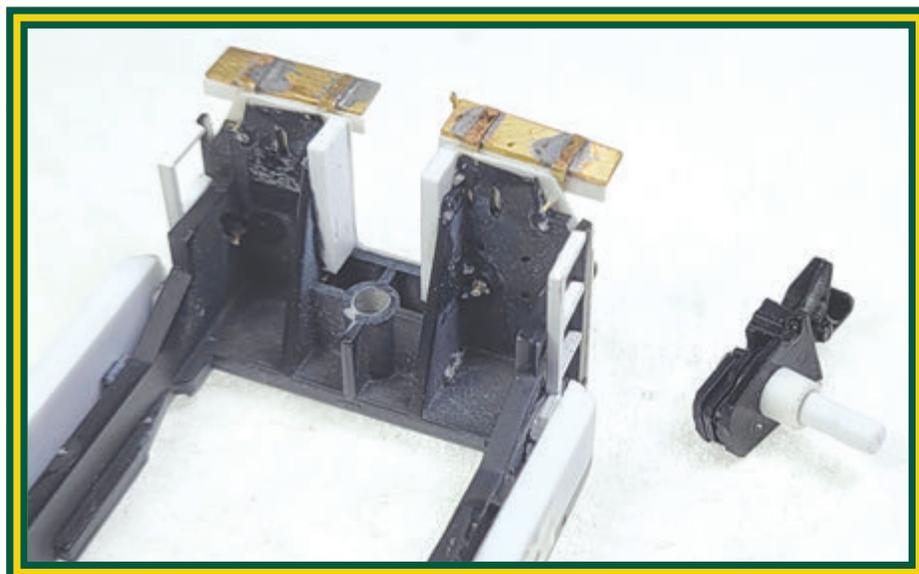
The model's lights are all LEDs mounted to two circuit boards, one in the roof of the cab and the other at the end of the long hood. There are individual LEDs for the four class lights, four number boards, and the headlight and backup light. All of them looked too yellow, so on both of our models they were replaced with golden white LEDs of the same size. The factory wiring was also a little odd to us as the

headlight/backup light function would also illuminate the class lights on that end of the locomotive, but there was a separate decoder function for the number boards. With a few cut traces on the circuit boards and the addition of a few jumper wires we got that changed around so that the number boards illuminated with the headlights and all four class lights were now together on another function. Each board required swapping the two function wires, a simple task. The slightly more challenging change was the traces. The long hood board required the trace below the headlight LED to be cut with a sharp knife. Then a jumper wire was soldered in place from the hole in the board by the "H" to the left side of

the headlight LED. On the cab board, the trace below the backup light was cut and the bottom of the three center LEDs were jumpered together. Like the long hood board, the two function wires had to be swapped as well.

## PAINT AND DECALS

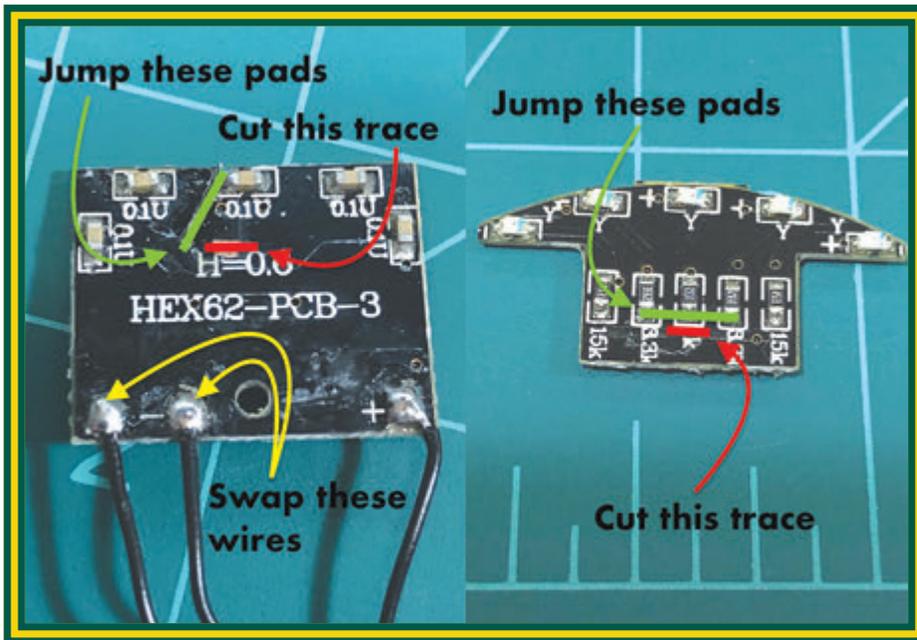
The prototype #81 wore the classic White Pass Rodeo Yellow and Alhambra Green paint scheme during its time in Alaska. Since Rob was modeling the locomotive as it appeared on the railroad, he painted his model in that scheme using



Above: The "Battleship" peg with a coupler is ready to mount to the body. The peg is a simple friction press fit. Styrene strip was used to narrow the frame's pocket width to better accept the Kadee #714 couplers, which have also been shortened so they won't interfere with the trucks. Photo by Rob Bell.



Left: The molding around the new lower windows on Rob's model was simulated with Tichy phosphor-bronze wire glued in place with ACC. Photo by Rob Bell.



Above: The circuit boards for the lighting on each end of the locomotive took some reworking to get the lights to behave how we wanted them to. This diagram shows where board traces were cut and where jumps were added. Photo by Mike May.

Floquil BN Green and Reefer Yellow, lightened a touch with white. The decals were made by me to Rob's specified height and printed by Draw Decals. Apple Barrel Brown Oxide craft paint and a tiny brush were used to paint the red primer in the approximate pattern the real #81 had after the White Pass added the wider walkways and long hood handrails. Rob weathered his model with Pan Pastel powders using photos of the prototype as a guide.

My model was painted in the solid blue Federal Industries scheme that the White Pass used in the last few years before the railroad closed in 1982, even though #81 was never dressed in this scheme. Tru-Color or GTW Blue is a very close match to the prototype blue. Decals are a combination of Republic Locomotive Works and some

Below: The right side of Rob's version of #81 ready for paint. The cab roof vent was made from bits of styrene rod. Photo by Rob Bell.



Right: The fireman's side of #81 on Rob's Skagway wharf. The "White Pass" lettering was custom designed by Mike to Rob's specifications. The "Safety" logo on the access door below the cab is a CDS dry transfer applied to clear decal paper and then applied to the model. Photo by Rob Bell.





Above: Fresh from the paint and weathering booth, Mike's #81 is ready for service on his layout. This model is certainly proto-freelanced but captures the essence of the prototype, especially had it stayed in service longer. Photo by Mike May.

custom ones again printed by Draw Decals. I weathered my model with Tru-Color paint washes, pan pastels, and airbrushing.

**COLLABORATION**

Building a representation of this unusual locomotive would have been a great project under any circumstances, but to collaborate in real time with another mod-

eler across the country made it that much more interesting and enjoyable. Sometimes it was the technical discussions of how to make something work, and other times just the fun of tossing ideas around. Many of us have been inspired by construction articles to build an identical or variation of a project that another of us in the modeling community has done, but to work at the same time was certainly something that I think we'll both be doing again. x

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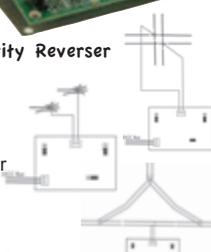


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# LOCOMOTIVES OF THE WHITE PASS & YUKON ROUTE

## Part 5: The Climax of the Story

by Rob Bell

The White Pass & Yukon Route was built to serve the gold seekers of the Klondike Gold Rush of the late 19th and early 20th centuries. The WP&YR was really four companies, each chartered in one of the territories, plus the construction company. The Pacific and Arctic Railway and Navigation Company was chartered to operate in the Alaska Territory (mile 0–20.4). The British Columbia-Yukon Railway Company was chartered for the operation of the section in British Columbia (mile 20.4–52.58). The British Yukon Railway Company was chartered in the Yukon Territory (mile 52.58–112.0). The Pacific Contract Company (PCC) was responsible for the construction of the entire WP&YR. The PCC acquired the first locomotives of the line second hand from several sources located in the Pacific Northwest, (Washington State and southwestern British Columbia). When construction was completed, all locomotives and equipment acquired by the PCC would officially become property of the WP&YR. Perhaps the most unique of all the Route's locomotives was the Climax #8.

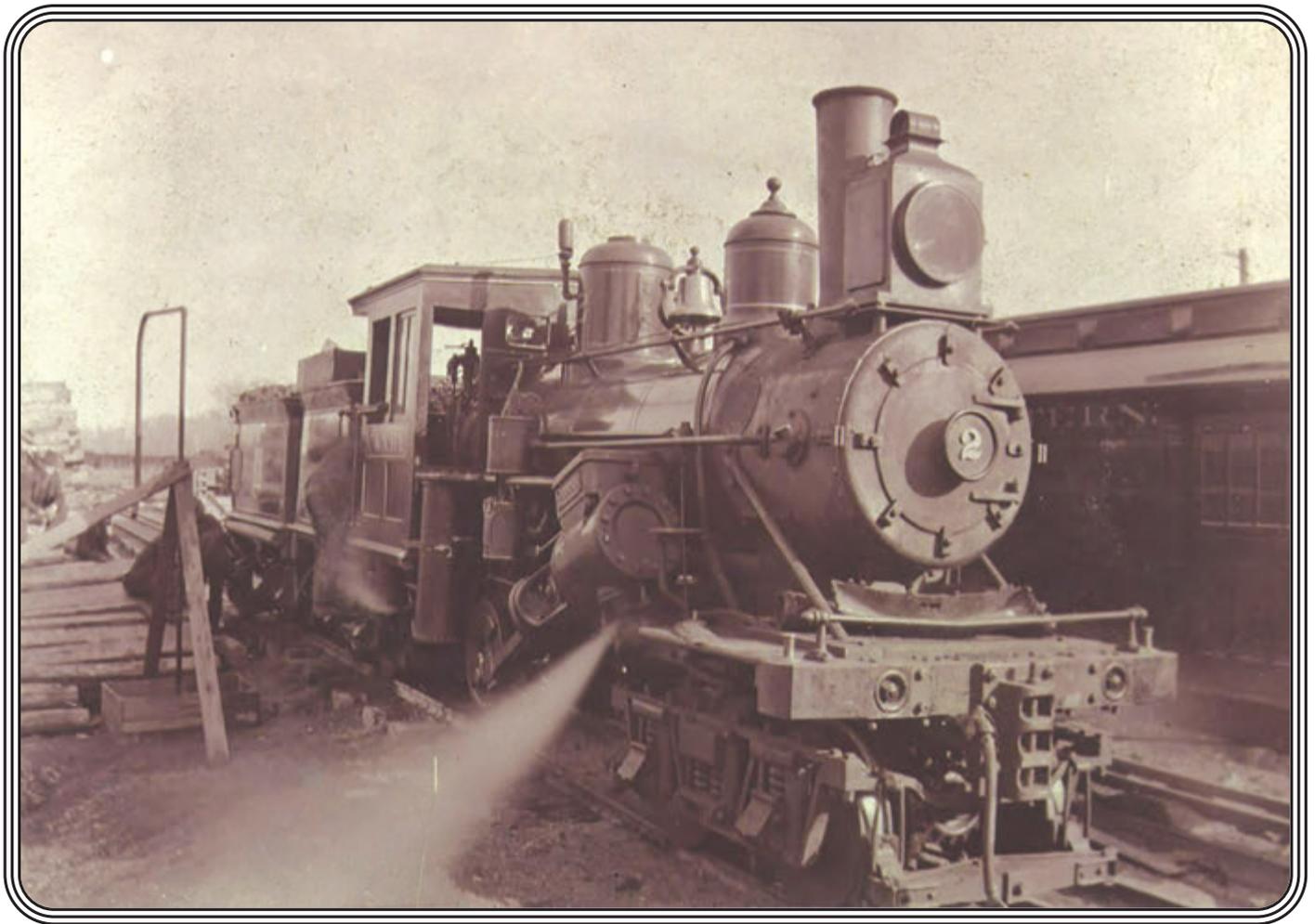
The story of #8 starts many years before the Klondike Gold Rush in the Front Range of the Colorado Rocky Mountains. Gold was discovered on Boulder Creek just

west of present-day Boulder, Colorado, in 1859. Numerous mining towns sprung up, and silver was discovered in the area ten years later. By 1880, Colorado was reportedly the leading gold and silver producing state and talk of railroads was in Boulder's air. The 3-foot gauge Greeley, Salt Lake & Pacific (GSL&P) was incorporated in January 1881 and was soon running 14½ miles up Boulder Creek to the mining town of Sunset, Colorado. Things went well until May of 1894 when spring rains and warm temperatures melted the mountain snows. Boulder Creek became a raging river washing out all the roads and railway bridges in Boulder Canyon, thus shutting the GSL&P down for good.

Citizens of Boulder sought to build another railway from Boulder to the various mining towns in the surrounding area and incorporated the Inter-Mountain Railway in April 1895. However, construction never began. The gentleman placed in charge of the inactive railway, M.F. Leach, proposed a tramway from Boulder to Ward, Colorado. He managed to get financial backing from New York and Pennsylvania sources and the Colorado & Northwestern Railway Company (C&N) was incorporated on July 22, 1897. The C&N took over all aspects of

Below: Colorado & Northwestern #2, a one-of-a-kind narrow gauge 3-truck Climax, somewhere between Boulder, Colorado, and Ward, Colorado. The shop number of this locomotive was #167. Photo was most likely taken in 1898 as the locomotive arrived in Colorado in late December or early January and was sold to the Pacific Contract Co. in January 1899. Photographer unknown, Bruce Pryor collection.





Above: Colorado & Northwestern Climax #2 is shown here in the Boulder, Colorado, yards ready for a trip up Boulder Creek. The locomotive is missing the pilot and the gear for the front axle can be seen. Photographer unknown, courtesy the Boulder Historical Society/Museum of Boulder, A.A. Paddock collection.

the inactive Inter-Mountain and construction began in August. The C&N did not follow the old GSL&P route, but rather a new grade was laid out on higher ground. The new route, slightly steeper with fewer trestles, was complete to Sunset, Colorado, in February 1898. The C&N continued laying ties and rail towards the mining town of Ward, Colorado. In late December 1897 or very early January 1898, the C&N took delivery of a most unusual 50-ton, three-truck locomotive from the Climax Locomotive Company of Cory, Pennsylvania.

Colorado & Northwestern #2 was the heaviest Climax geared locomotive built up to that time and was the only narrow gauge three-truck, class "C", Climax ever built. Class C Climax locomotive cylinders were inclined about 30 degrees on each side of the boiler, driving a cross-shaft which was geared to a driveshaft positioned centrally under the boiler. This driveshaft was in turn geared to each axle on each truck using universals and bevel gears. The results were a locomotive that had tremendous tractive effort and an ability to operate around sharp curves and on steep grades albeit at a slow pace.

The C&N purchased #2 to handle heavy

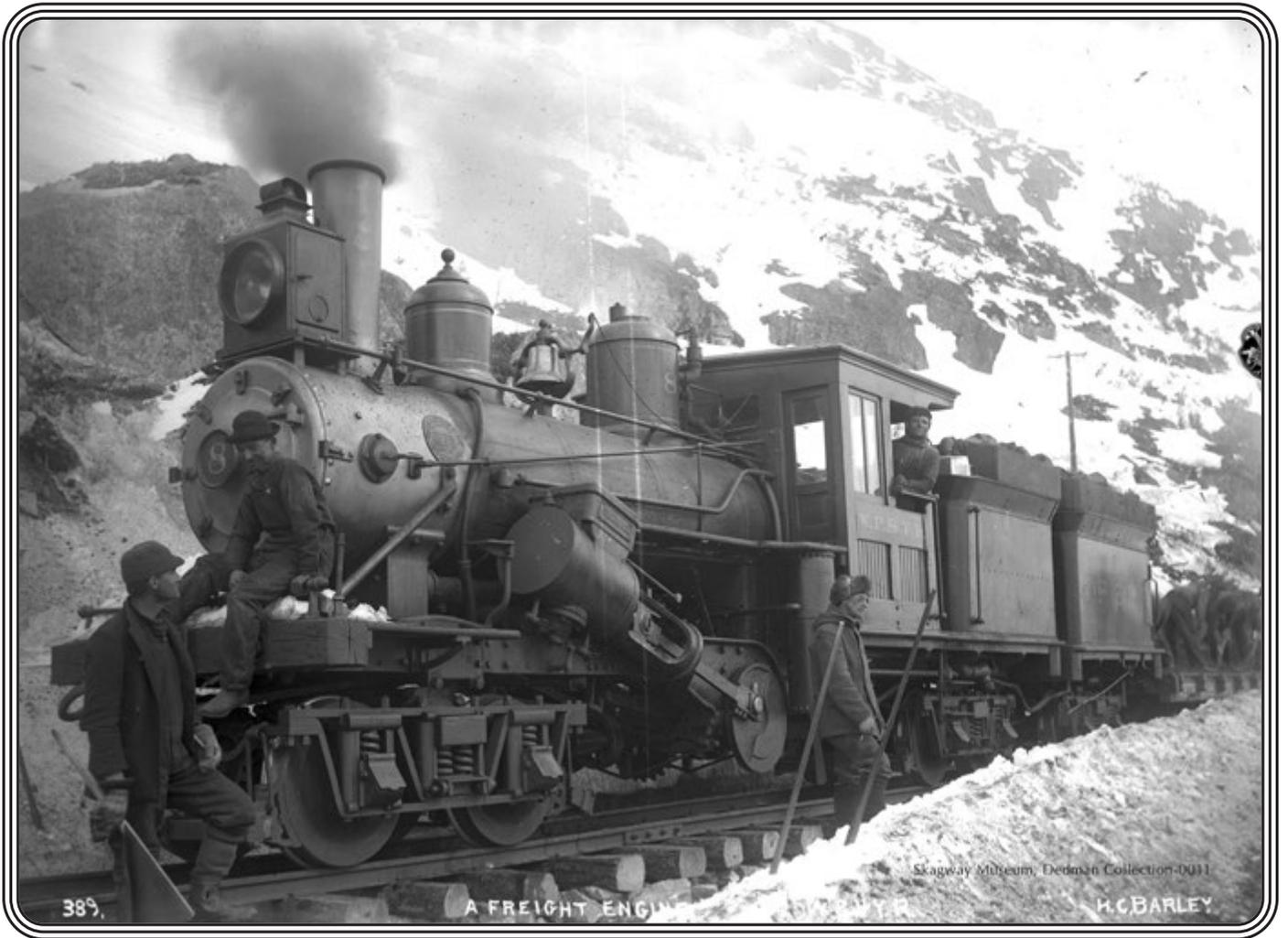
supply trains up to Ward, Colorado, and the heavy ore tonnage from Ward to the Culbertson Mill a few miles east of Boulder. The Climax didn't last long on the C&N as the line sold her in January 1899. No official reason was given for the sale, but Forest Crossen claims that "the crews knew one reason — too many derailments" in his book *The Switzerland Trail of America*. No matter the reason or reasons, the C&N sold the Climax rather quickly after acquiring it.

The White Pass & Yukon Route was seven months or so into construction when the Pacific Contract Co. acquired the Climax from the Colorado & Northwestern for \$3500.00. The crews on the White Pass apparently found the Climax quite capable on the steep and twisting rails out of Skagway. The Pacific Contract Company reported to the Climax Locomotive Company that they had "heard some good reports of the Climax Locomotive from our operating department at Skaguay. On the trial trip she pushed five loaded cars and a locomotive up our steep grades and sharp curves for a distance of ten miles, to the gratification of our people." (Note the early way of spelling "Skagway").

We know from photographic evidence

that #8 was used mostly for construction trains, but also helped with bucking snow. Additionally, we know that Climax #8 was present when rails reached Bennett in early July 1899. The entire 110-mile railroad was completed in July of 1900 when all the locomotives and rolling stock officially became the property of the WP&YR. Number 8 was not re-lettered physically to #58 in 1900 when the White Pass & Yukon Route renumbered the rest of the locomotives similarly. It may never have been renumbered to #58 even on paper as a 1905 report referenced the locomotive simply as

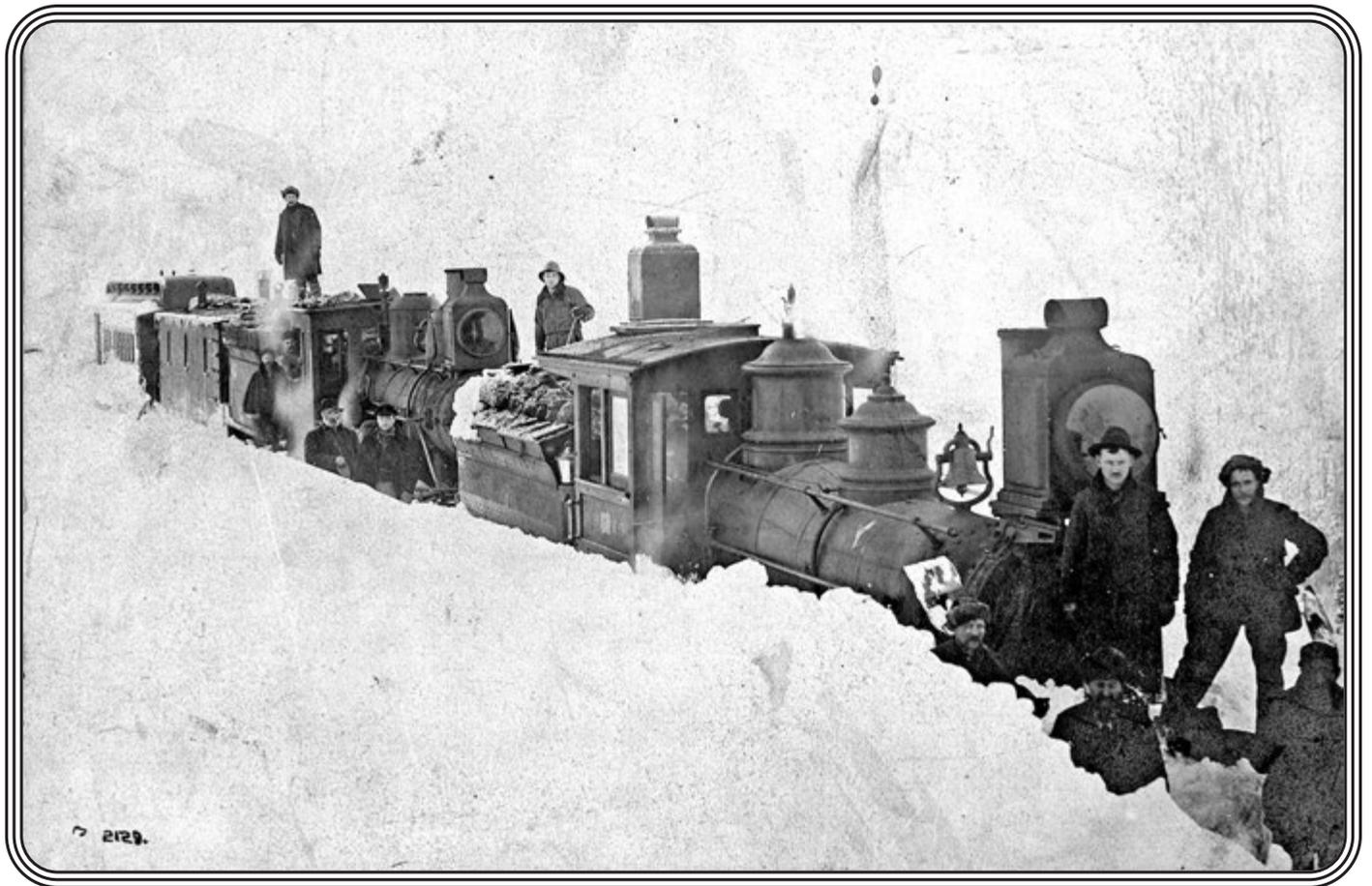
(text continued on page 41)



Above: White Pass & Yukon Route #8 pauses with at least one flatcar behind loaded with men that appear to be shoveling something: possibly ballast. The track has ties that are faced only on the top and bottom and is not ballasted yet. The locomotive does not have a pilot or footboards, but it does have extension boards to increase the coal capacity. Photo by H.C. Barley, courtesy Skagway Museum, Dedman Collection DCP0011, high resolution scan by Chuck Morse.



Right: WP&YR #8 and her crew take a break with four flatcars in tow. The locomotive has footboards front and rear installed now, and the track has proper ties and ballast. One member of the crew can be seen heading into the brush just below the front-left of the locomotive's end beam. Photographer unknown, courtesy Skagway Museum, Talbot Collection 067, high resolution scan by Chuck Morse.



(text continued from page 39)

“Climax 8.”

The Climax remained on the property only until June of 1902 when the W.D. Hofius & Co., a dealer in iron, steel, machinery, and assorted railway supplies located in Seattle, Washington, paid the WP&YR \$3000.00 for the locomotive. Sometime in 1903, Hofius sold the Climax to the White Brothers for use on their White Star lumber Company line out of Whites, Washington, as their #1. Either Hofius or the White Brothers removed the third truck and converted the locomotive to standard gauge. There has not been any good evidence that the Climax ever went to the Maytown Lumber Company as has been reported by some sources. The locomotive was probably scrapped around 1916, but we just don't know at this time.

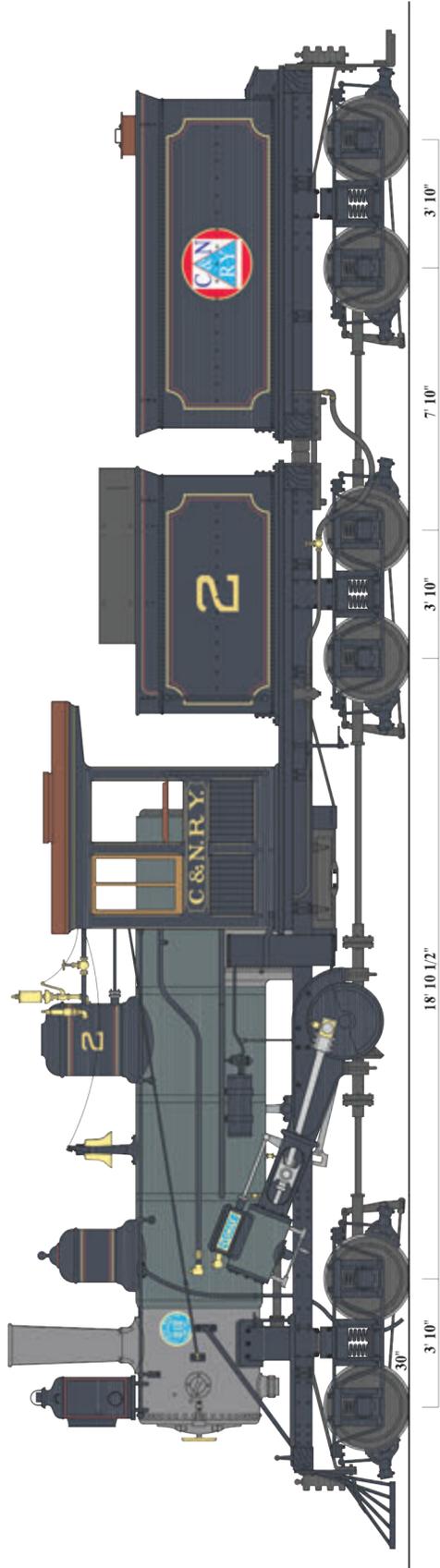
This is the story of a one-of-a-kind narrow gauge, three-truck Climax that had her roots in the Colorado Rocky Mountains and the associated gold rush there. The locomotive also saw service because of the Klondike gold rush in the Alaska Territory and nearby Canada. The Climax was apparently more successful on the WP&YR rails in southeast Alaska, yet she didn't last long there either. She ended her days in Washington state hauling logs and lumber in obscurity, her final days lost to the mists

of time. While the story of this locomotive is anti-climatic, however this is the story of the Climax of the White Pass & Yukon Route.

The accompanying drawings by David Fletcher of this Climax aren't the first to be done; but I believe them to be the most accurate to date. Drawings of this locomotive appeared in the September 1964 Model Railroader. However, that drawing had many errors including headlight mounting/position and the wheelbase of the third truck. Another drawing of this locomotive appeared in the November/December 1993 GAZETTE: the most notable issue with this one being the position of the third truck under the tank. A close study of prototype photographs and comparison of the different drawings and I think you will agree that David's are the most accurate so far.

Once again, I would like to acknowledge the contributions of Boerries Burkhardt, David Fletcher, Robert Hilton, Chuck Morse, Bruce Pryor, and John Stutz for their assistance. Next time, I will explore the White Pass & Yukon Route's main class of motive power for forty years, accompanied by more drawings from David Fletcher. x

Above: White Pass & Yukon Route Climax #8 and WP&YR #4 (a Baldwin 4-4-0, see part 3 of this series) take a break from plowing through some deep snow somewhere on the line. The snow was often deep enough that the only way to clear the rails was to employ men with shovels to reduce the depth to a point where the locomotives could handle it. Many times, this was the case even after the arrival of the big rotary snowplows. Photographer unknown, Klondike Gold Rush National Historical Park (KLG0), high resolution scan by Chuck Morse.



3' 10" 18' 10 1/2" 3' 10" 7' 10" 3' 10"

## COLORADO & NORTHWESTERN Rwy No. 2

CLIMAX MANUFACTURING Co., CORRY PENNSYLVANIA

CONSTRUCTION No. 167, DECEMBER 1897

SOLD TO WP&YR AS No. 8, 1899

FINAL DISPOSITION: TO WHITE STAR LUMBER Co., 1903

36" GAUGE

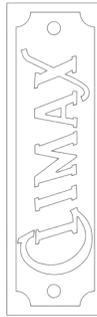
LIVERY - SPECULATIVE

DRAWING RECONSTRUCTED FROM WP&YR FOLIO DIAGRAMS

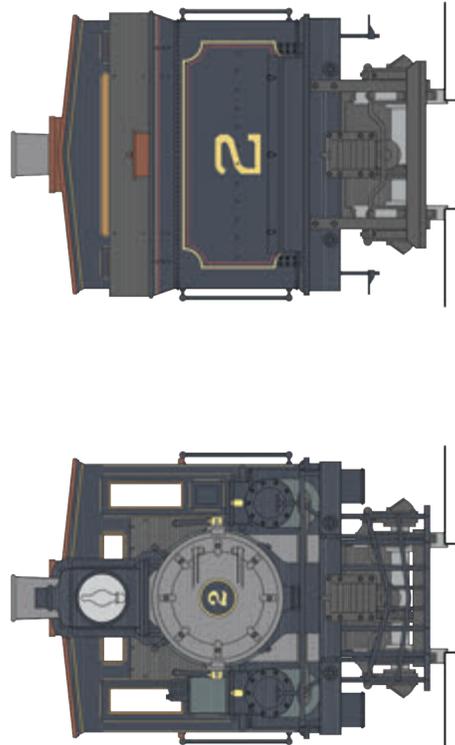
CLIMAX No. 8 DIAGRAM AND CLIMAX NO.58 DIAGRAM

WHITE PASS & YUKON ROUTE

ASSISTED WITH DRAWING BY J. HAROLD GEISSEL

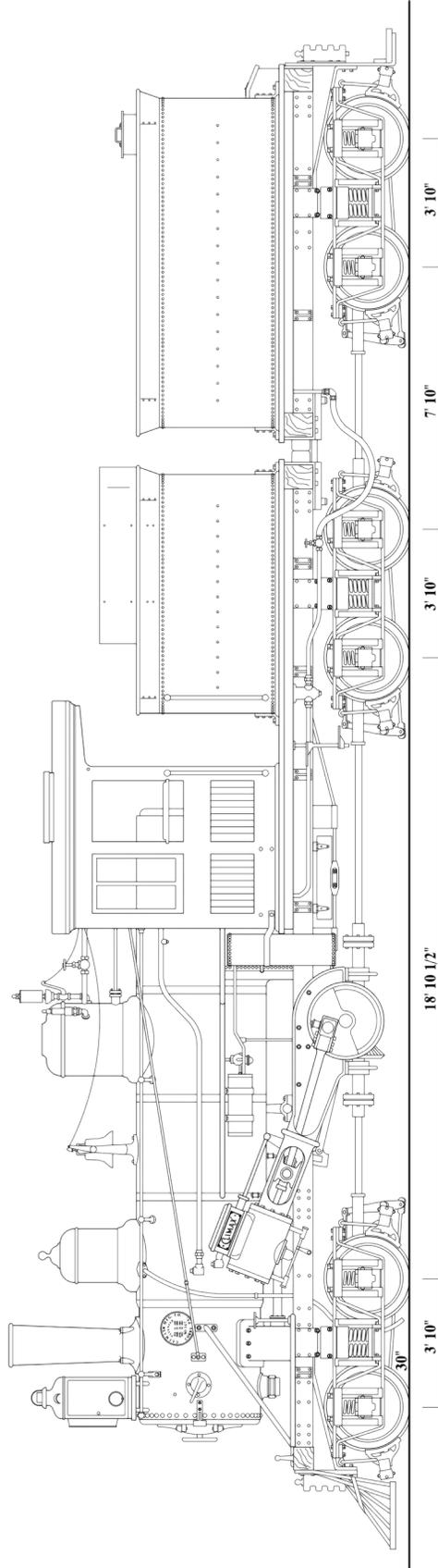


0 5 10 ft DRAWN BY DAVID FLETCHER  
SCALE: 3/16 INCH = 1 FOOT



SPECIAL THANKS TO ROB BELL FOR FOR DATA ASSISTANCE

DAVID FLETCHER 2022



## COLORADO & NORTHWESTERN Rwy No. 2

CLIMAX MANUFACTURING Co., CORRY PENNSYLVANIA

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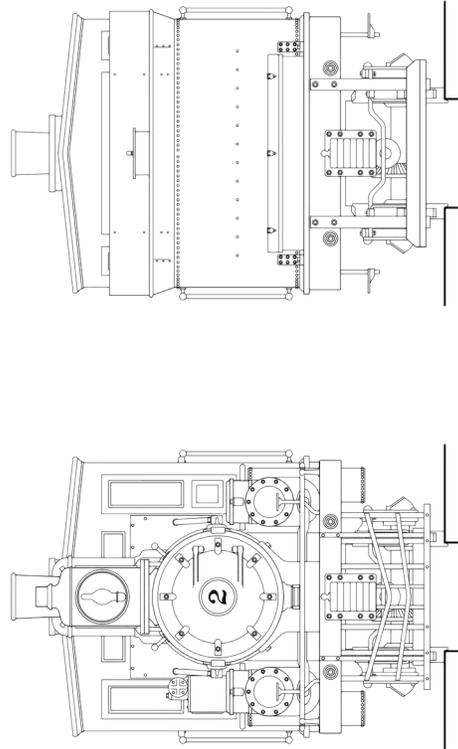
LIVERY - SPECULATIVE

DRAWING RECONSTRUCTED FROM WP&YR FOLIO DIAGRAMS

CLIMAX No. 8 DIAGRAM AND CLIMAX NO.58 DIAGRAM

WHITE PASS & YUKON ROUTE

ASSISTED WITH DRAWING BY J. HAROLD GEISEL



SPECIAL THANKS TO ROB BELL FOR FOR DATA ASSISTANCE

DAVID FLETCHER 2022

# 42nd NATIONAL NARROW GAUGE CONVENTION CONTEST WINNERS



Courtesy of Robin Peel

Here is a list of winners and photos of first place winners at the 42nd National Narrow Gauge Convention held in Tacoma, Washington, September 1-4, 2022. As in past conventions, the winners were chosen by popular vote, with the exception of the Special Awards. This committee chose not to have special categories for black and white or color photos.

The contest was chaired by Dave Woodrell and Dennis Terpstra. I want to thank Convention Chairman, Robin Peel, for the best organized list of



## MODEL CONTEST

### BEST IN SHOW AND FIRST PLACE SQUARE FOOTER

Brian Block — Lobster trap & bait shop (square footer), 1:48 scale

### STEAM MOTIVE POWER — ROD

First: Don Rose — D&RG K-27 No. 463, 1:20.3 scale

Second: George Bogatiwik III — General in 1862 — HO

### STEAM MOTIVE POWER — GEARED

First: Chuck Lind MMR — Tall Pine Lumber Company #6 Shay, On3

Second: Russ Segner — 16 ton Shay Coal Creek Lumber Company, Sn3

Third: Mark Allen — NCTC 2 cylinder Shay, On30

### MOTIVE POWER — INTERNAL COMBUSTION

First: Mark Allen — Mack yard switcher — On30

Second: Mark Allen — Motor #7 Galloping Goose, On30

Third: Phil Holden — Fire speeder, On30

### SPECIAL EQUIPMENT

First: David Dewane — Pile driver on disconnects with trailing car, On30

Second: George Chambers — Logging railroad pile driver, O

Third: Chuck Lind MMR — Formanek Timber Mack Bulldog Truck

### MAINTENANCE-OF-WAY

First: Ben Popper — Pine Creek Lumber Company flanger caboose No. 12, On30

Second: Mark Allen — Fire Goose, On30

Third: Chris P. Rogerson — Fire car, On30

### FREIGHT CAR

First: Brian Block — Donkey on two disconnects, On3

Second: Bob Harper — Sandy River 33-foot pulpwood flat #373, On2

Third: Phil Holden — Flatcar with wooden water tank, On30

### PASSENGER CAR

First: Dave Adams — D&RGW second named "Durango" parlor car, 1/4" to 1-foot scale

Second: Mike May — White Pass & Yukon car #211, HO3

Third: Marc LaChey — Cripple Creek & Tin Cup rider gondola, Sn3

### CABOOSE

First: Hugh Blaney — D&RGW 0577 short caboose, On3

Second: Chuck Lind MMR — Tall Pine Lumber Company #2, On3

### FAVORITE TRAIN

First: Hugh Blaney — D&RGW maintenance of way circa 1939, On3

Second: Steve Bradley — Southern Pacific narrow gauge mixed train, HO3

Third: Don Railton — Gravel train, Gn15

### STRUCTURE

First: David W. Fritsche — Gas station, O scale

Second: Alan Murray — Port Townsend Southern depot, O scale

Third: Dennis Reeve — Warehouse, N scale

### DIORAMA

First: Randy Meyer — Truck repair, O scale

Second: Chuck Lind MMR — Abandoned sawmill, On3

Third: Chuck Lee — Majestic mine, HO

### MODULE

First: Al Jones — Moose Creek Logging Company, On30

Second: Charles Proudfoot — Palisades module, HO3

Third: Karl Kleeman — Monson RR, On30

### SQUARE FOOTER

First: Brian Block — Lobster trap & bait shop (square footer), 1:48 scale

Second: Mike Engler — 1900s brass & iron foundry, HO

Third: Stephen Drees — Deer Creek mine, HO

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## SPECIAL AWARDS

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### MT. ALBERT SCALE LUMBER AWARD

David Deware for his On30 pile driver on disconnects

### BEST USE OF BACHMANN On30 OR LARGE SCALE

Don Rose for his 1:20.3 scale K-27 #463

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## PHOTO CONTEST

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### MODEL PHOTOGRAPH

First: Bob Treat — Shay on Trestle

Second: Bob Treat — Looking at Moose

Third: Bob Treat — Snow Creek Town

### PROTOTYPE PHOTOGRAPH

First: Bob Treat — Gondola Ride

Second: Bob Treat — Chama Yard at Night

Third: Barry Dupler — 480 Making Steam



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B E S T I N S H O W

---



**BEST IN SHOW and  
FIRST PLACE SQUARE FOOTER**

Brian Block's O scale lobster trap and bait shop

F I R S T P L A C E A W A R D S



ROD LOCOMOTIVE

Don Rose's 1:20.3 scale D&RG K-27 #463 (also Best Use of Bachmann On30 or Large Scale Model)



GEARED LOCOMOTIVE

Chuck Lind's, (MMR) On3 Tall Pine Lumber Company Shay #6

INTERNAL COMBUSTION LOCOMOTIVE

Mark Allen's On30 Mack yard switcher



# F I R S T P L A C E A W A R D S

## MAINTENANCE-OF-WAY

Ben Popper's On30 Pine Creek Lumber Company flanger caboose #12



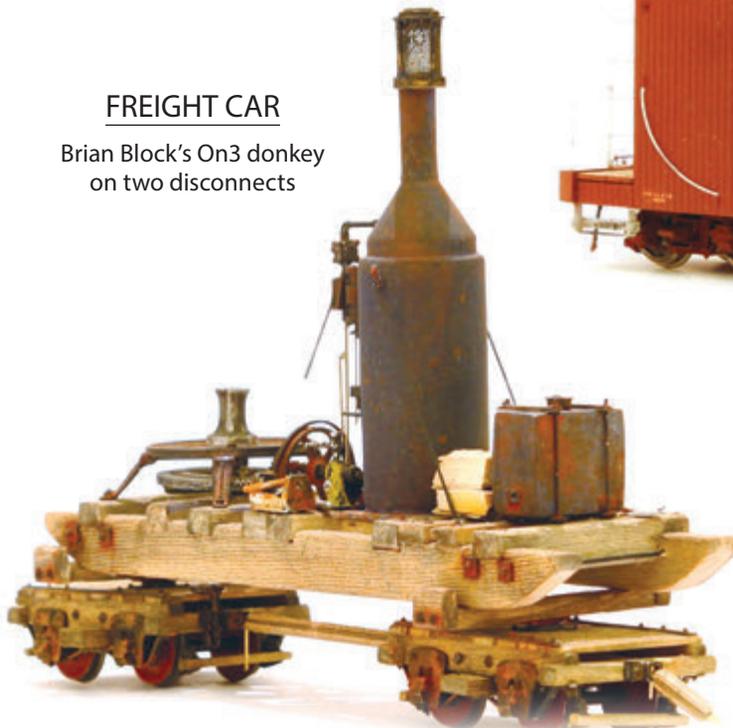
## PASSENGER CAR

Dave Adams' On3 D&RGW second named Durango parlor car



## FREIGHT CAR

Brian Block's On3 donkey on two disconnects



## CABOOSE

Hugh Blaney's On3 D&RGW #0577 short caboose

# F I R S T P L A C E A W A R D S

## COMPLETE TRAIN

Hugh Blaney's On3 D&RGW 1939 circa maintenance-of-way train



## SPECIAL EQUIPMENT

David Dewane's On30 pile driver on disconnects with trailing car (also Mt. Albert Scale Lumber Award)



## STRUCTURE

David W. Fritsche's O scale gas station

# F I R S T P L A C E A W A R D S



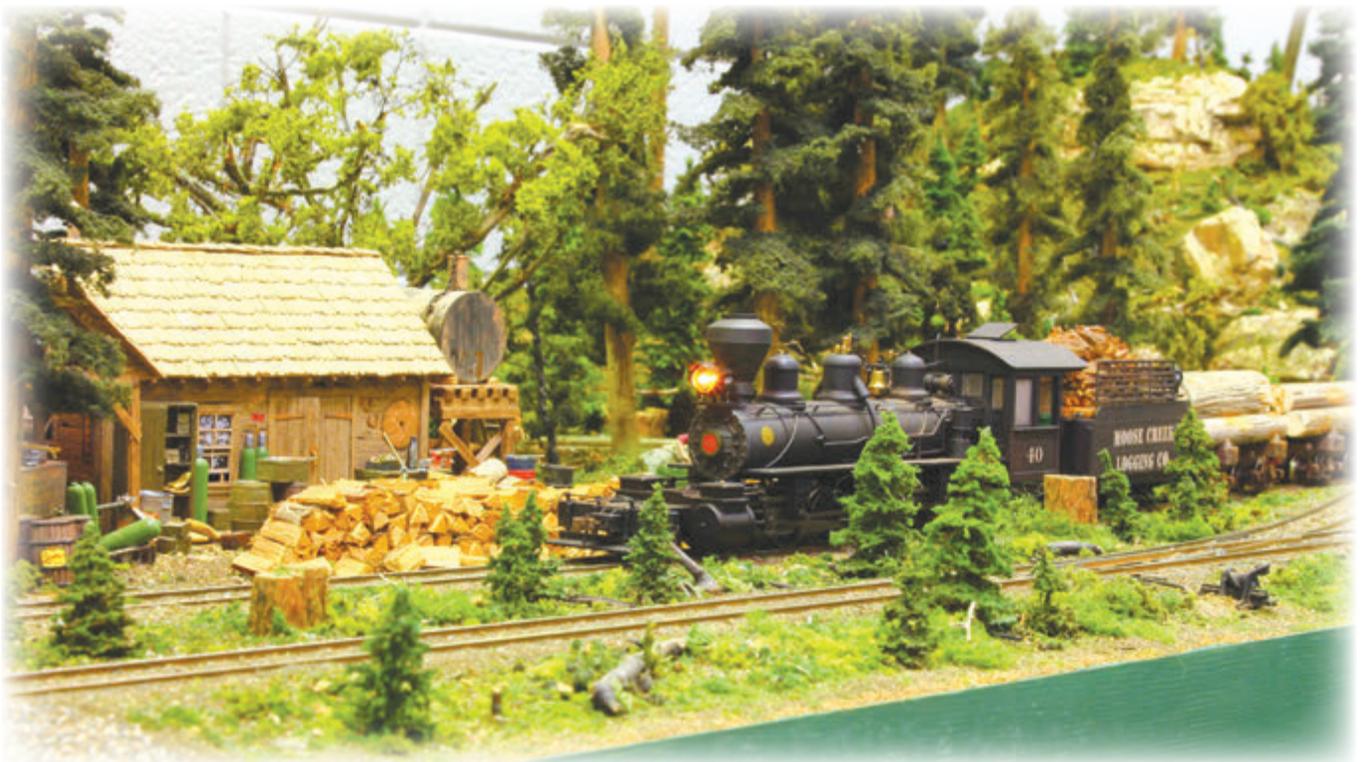
## DIORAMA

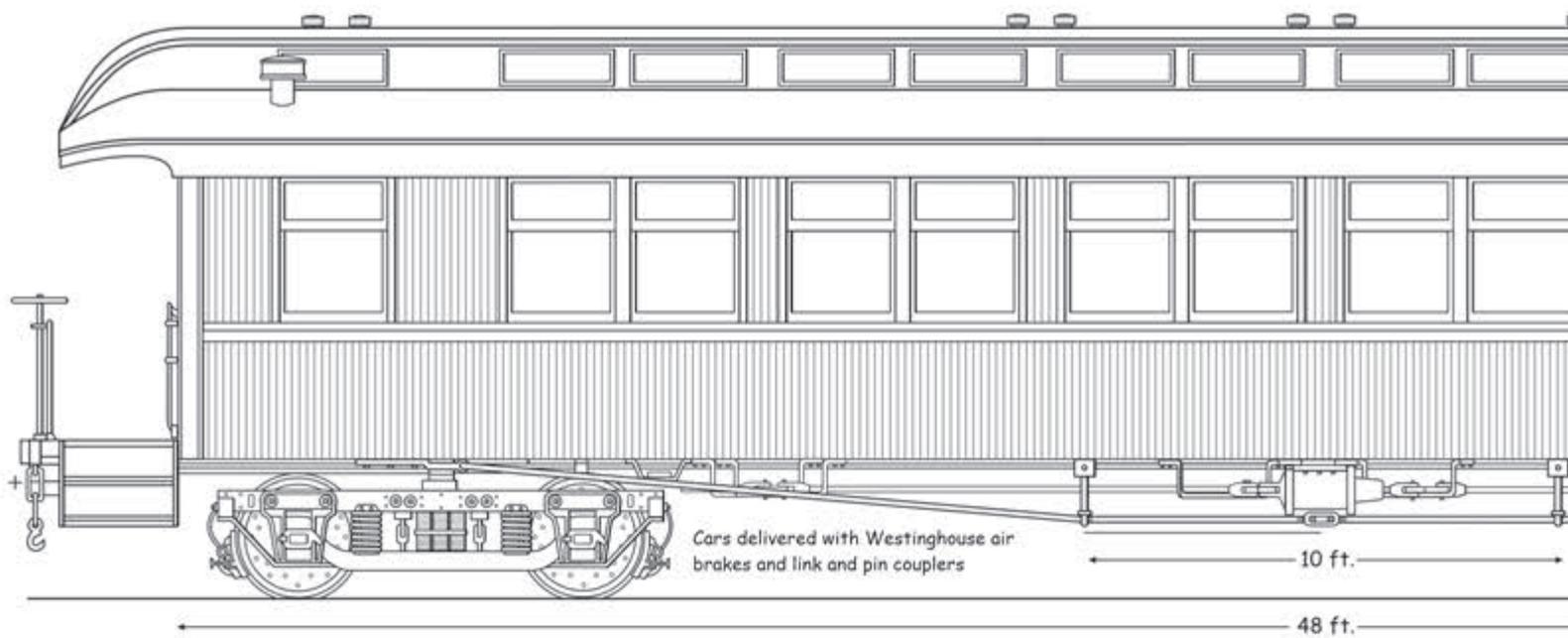
Randy Meyer's O scale truck repair



## MODULAR LAYOUT

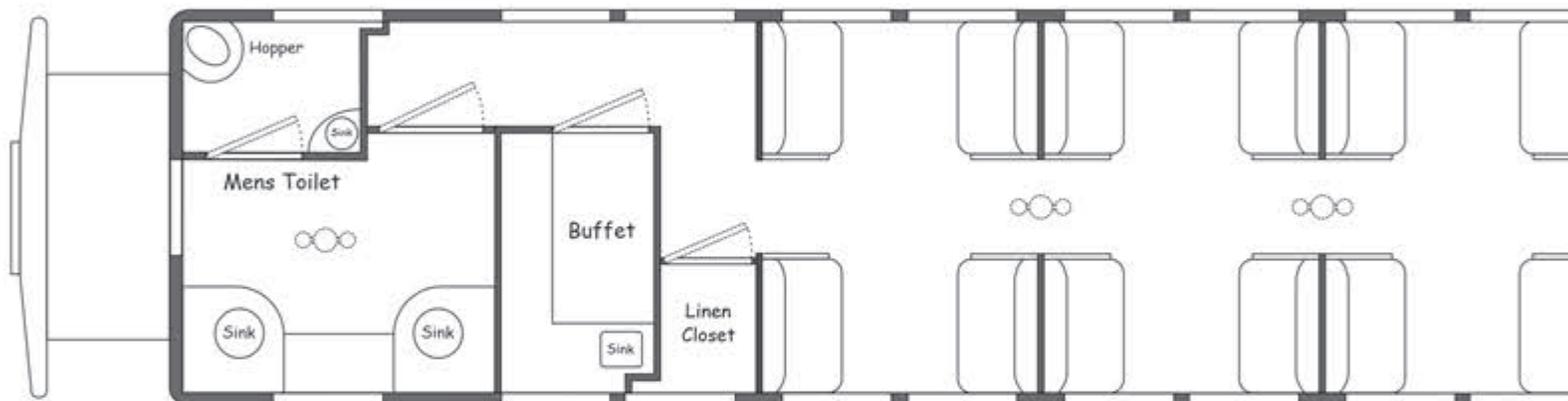
Al Jones' On30 Moose Creek Logging Company





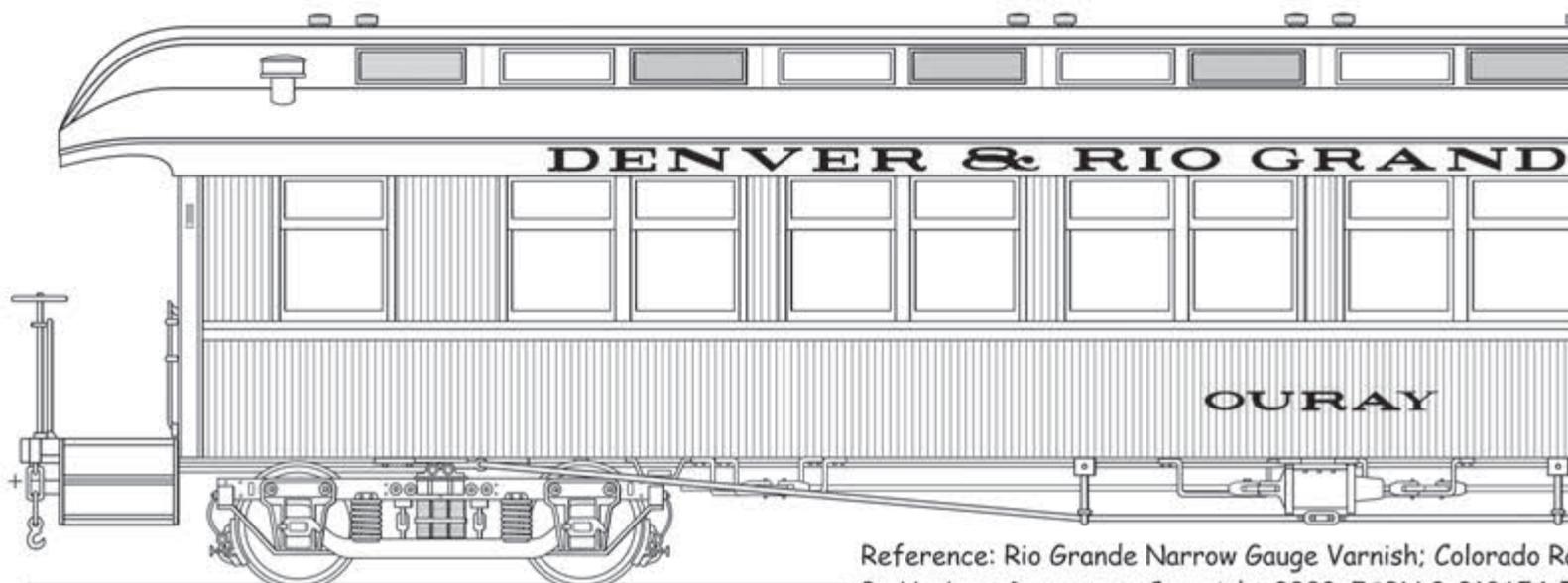
**DRAWINGS BY ROBERT STEARS © 2022**

As delivered configuration of 1883 Pullman 48 ft. buffet/sleeper. Original decoration of these buffet/sleepers is unknown. Color of cars was probably maroon or dark plum with gold colored lettering.

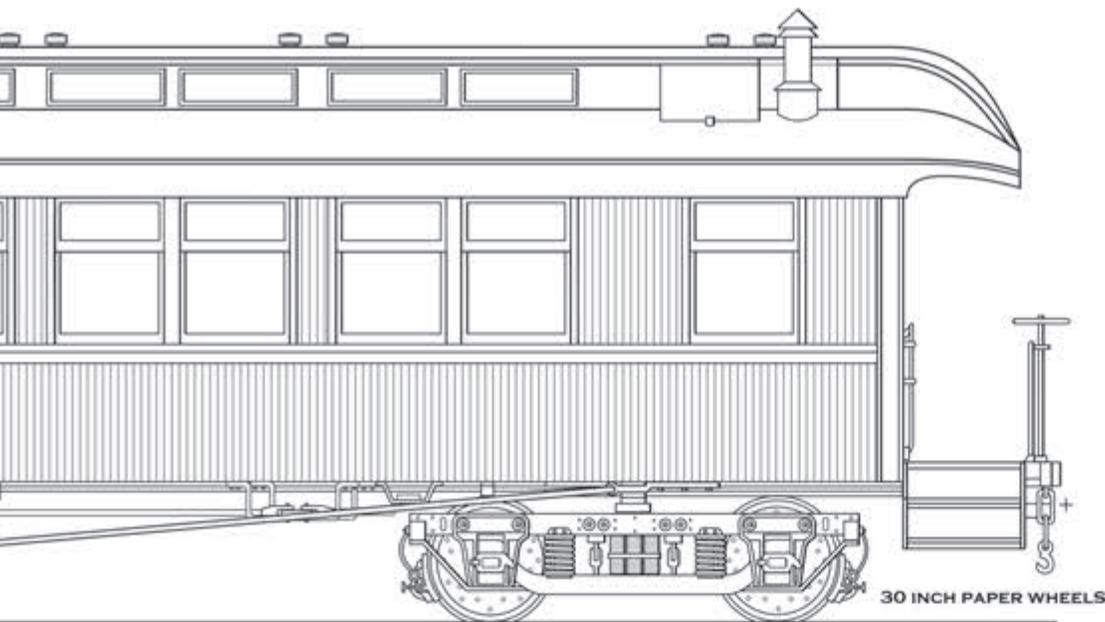


Interior arrangement of sleeping berths and buffet as delivered to

Appearance of *Ouray* in 1932

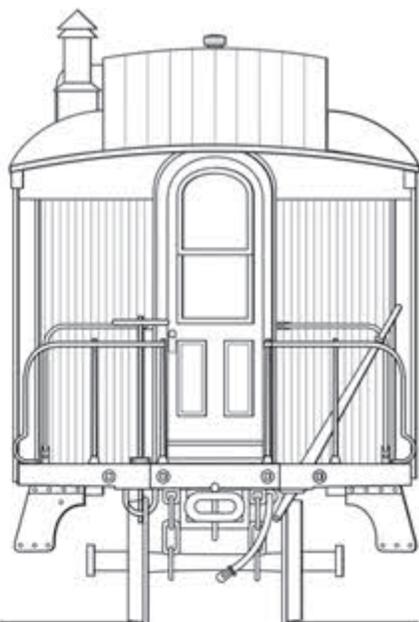


Reference: Rio Grande Narrow Gauge Varnish; Colorado Railroad Museum. By Herbert Danneman. Copyright 2003. ISBN 0-918654-7-



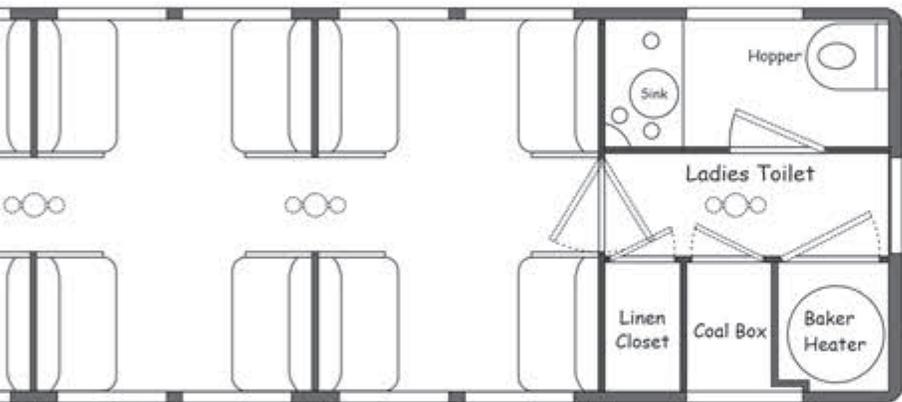
30 INCH PAPER WHEELS

SCALE: 1/4 INCH = 1 FOOT



3 ft.

ing cars.  
of these  
ettering.



the Denver & Rio Grande in 1883

**PULLMAN NARROW GAUGE  
PLAN 178 SLEEPING CARS.**

**48 FOOT LONG NARROW GAUGE PULLMAN  
SLEEPING CARS ASSIGNED TO THE DENVER &  
RIO GRANDE IN 1883.**

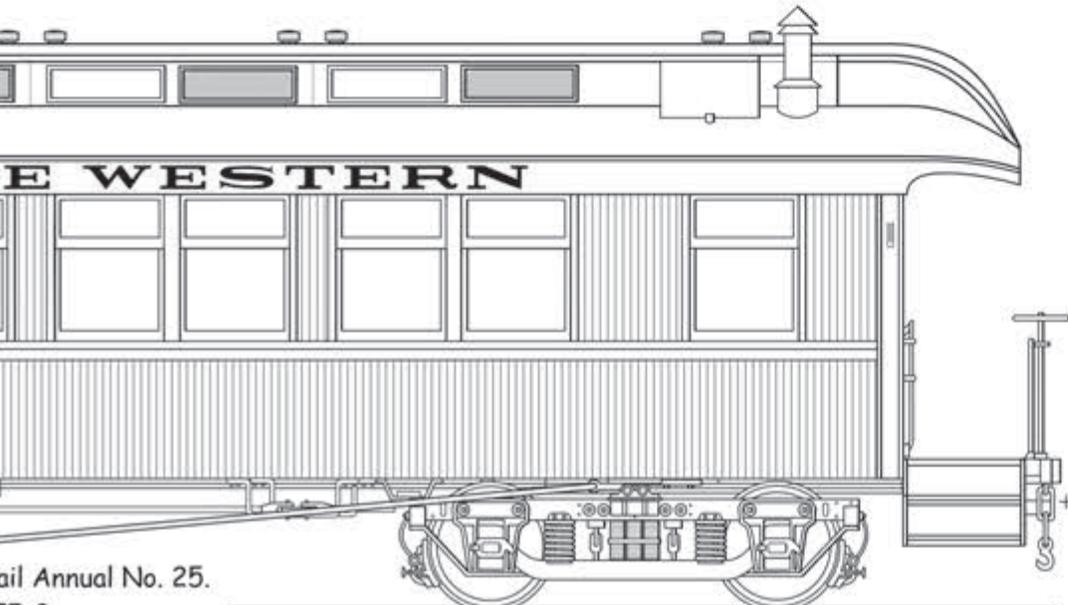
In 1883 the Denver & Rio Grande received six ten section/buffet sleeping cars from Pullman built to Plan 178 (lot 211). These 48 foot long sleepers were used on the D&RG narrow gauge "transcontinental" route between Denver, Colorado and Ogden, Utah.

The six Plan 178 buffet/sleeping cars were originally named *Castle Gate*, *Cimarron*, *Ogden*, *Provo*, *Salida* and *Salt Lake*.

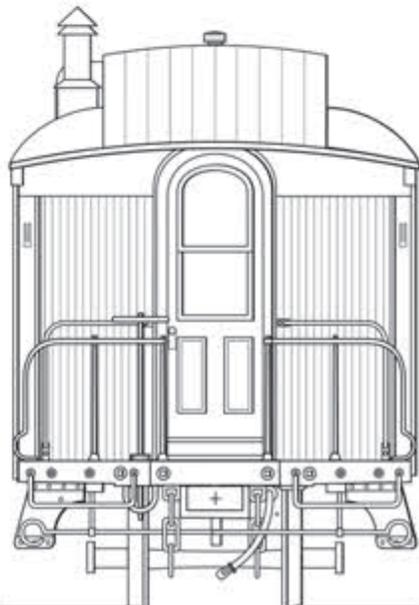
In 1899, buffet/sleeper *Salt Lake* was renamed *Ouray*. In 1901, *Ouray* was wrecked on Marshall Pass and purchased by the D&RG in 1902. The D&RG rebuilt the *Ouray* into a 48 seat parlor car in 1902.

In 1923 the car body and platforms were lowered, the underframe reinforced and extension side bearings installed.

*Ouray* was dismantled in Alamosa in April, 1938.



ail Annual No. 25.  
75-0



3 FT.

# YEON & PELTON COMPANY INCLINE

## A 42-INCH GAUGE RAILROAD IN OREGON

by Peter J. Replinger

The Yeon (pronounced “yawn”) & Pelton rail line, located about three miles below the town of Rainier, Oregon, was a 42-inch gauge railroad known as “bastard gauge” by loggers. Most 42-inch logging railroads came in existence because the operators chose initially to purchase used motive power that had been used on the Portland, Oregon street railways. Most of these street railways were 42-inch gauge and used steam “dummies” for motive power. Initially the street railways used horses for motive power which couldn’t be used for suburban service, nor were they too practical for hilly routes. For this service, the operators turned to the tried-and-true steam locomotive. The locomotive was covered with a coach-style body, and the exhaust was heavily muffled. Still, the public accepted them with some reluctance. These “dummies” remained in service until the early 1900s when the lines were electrified. The old

“dummies” were then sold for use on various northwest logging railroads. The Yeon & Pelton railroad was no exception, having acquired three of these type locomotives from a previous operator.

In January 1901, and earlier, John B. Yeon was logging on the Flokamin River near Cathlamet, Washington, using a splash dam. By March of that year, he sold his camp and timber to the Pelton-Armstrong Logging Company, including 30 million board feet of timber, for \$40,000.00 This gave them control of one of the best logging streams on the Columbia River. The Columbia River and Oregon Timberman Magazine referred to J. B. Yeon as “the well-known retired logging capitalist of Cathlamet, Washington.”

By December 1901, J. B. Yeon purchased the interests of Star Logging Company of Rainier, Oregon, for \$74,000.00 The property consisted of 5,160 acres of timber, 4.5 miles of 42-inch gauge rail-

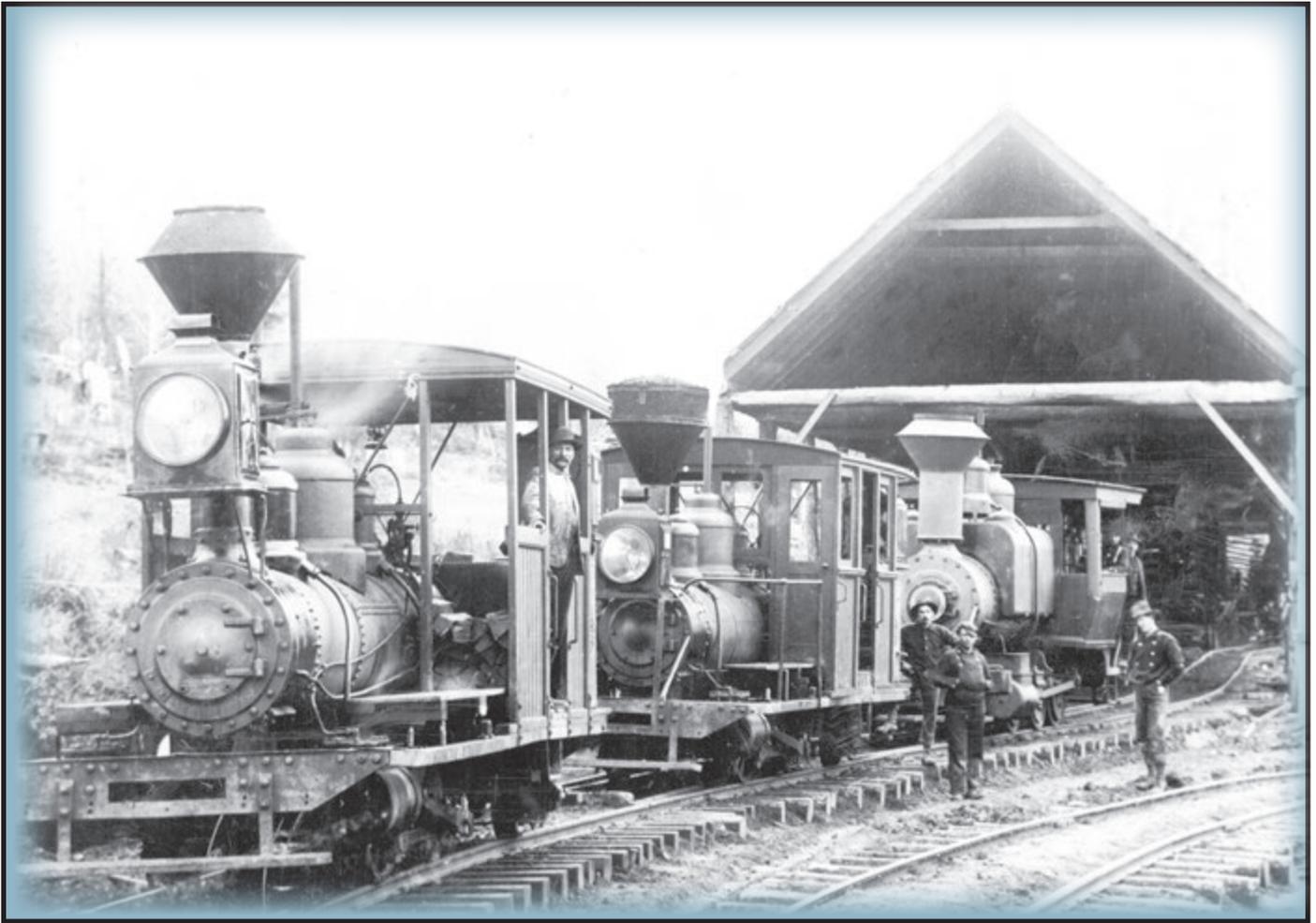
road, three locomotives and four donkey engines. The Star Logging Company was started in 1897 to log heavily timbered sections in the rugged hills south and west of Rainier. Their timberlands were in what was known as the Rinearson Tract, a vast expanse of old growth forest in the Beaver Creek watershed.

They built a log chute from top of the bluff to Rinearson Slough, 600 feet below. In June 1897, work was begun on a four-mile railroad from the head of the chute to their timber along Beaver Creek. For motive power, three steam “dummies” were purchased from a Portland street railway, the Willamette Bridge Company, which

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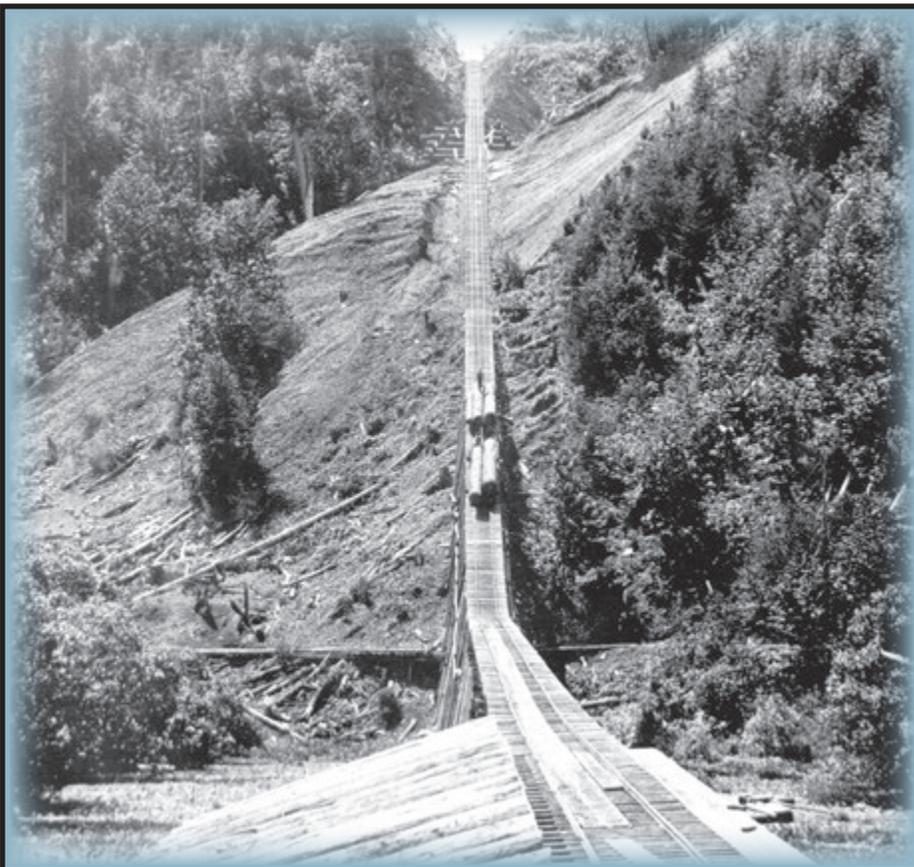
Below: This photo shows the configuration of the steam “dummies” when they were used to pull street cars in the City of Portland. Merv Johnson collection.





Above: Yeon and Pelton's lineup of rod engines. Front to rear is #3, Baldwin number 9973, built in April 1889; #2, Baldwin number 9847, built in February 1889 and #4, Baldwin number 10683, built in March 1890. The building in the rear is where the loads were prepared for the trip down the incline. John Labbe collection.

Left: Two loaded cars being lowered down the incline, taken from the log dump. John Labbe collection.



was in the process of electrification.

In January 1902, Yeon incorporated the Yeon and Pelton Company of Portland for \$150,000.00. J. B. Yeon was listed as president; David C. Pelton as vice president and treasurer; and J. F. Boothe as secretary. By April 1902, they also bought timber and land in the Rainier area from three different individuals. Yeon had concluded that almost fifteen percent of the logs that Star Logging Company had sent down their

(text continued on page 56)



Left: Three loads of logs on their way down the incline to Rinearson Slough to be dumped with spring freshet flooding lowlands. It appears that the loggers riding on the loads were used to unload cars by hand and then coil rigging back on the cars. Merv Johnson collection.

Below: Locomotive taking empty disconnected cars from top of incline with three loads hooked to incline cable ready to go down. John Labbe collection.





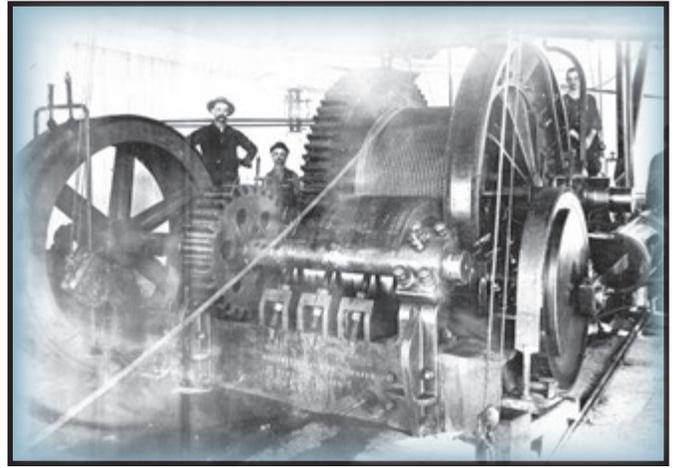
Above: The Yeon and Pelton 2-spot moving a steam donkey. In the foreground next to the stump is John Yeon. John Labbe collection.



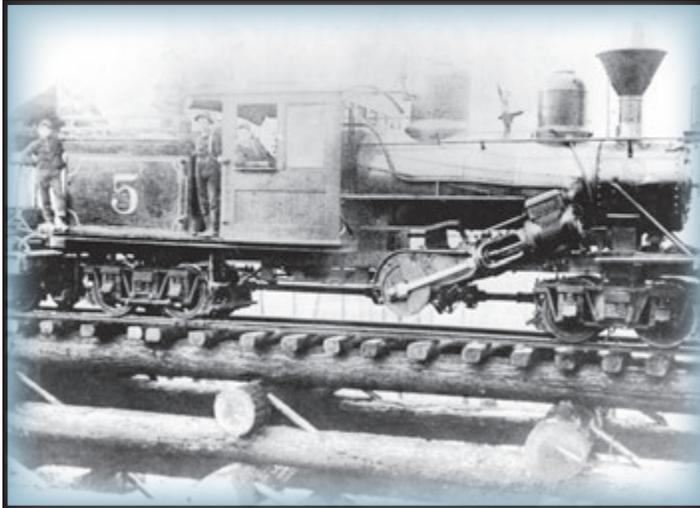
Left: The rollway "landing" with a yarding donkey being used to yard a fir log and a locomotive waiting for disconnected cars to be loaded. Note, this system holds logs on rollway before loading them by hand. Merv Johnson collection.

(text continued from page 53)

chute were damaged beyond use for the manufacture of lumber. To eliminate this waste, Yeon devised a half mile railroad incline to run parallel to Star's log chute. When it was completed in 1902 at a cost of \$19,000,00 it was the first incline in the Columbia Basin, and beyond, and was considered a marvel. Lumbermen traveled from across the United States and Canada to see it in action. Most of the 2,400-foot



Above: The second incline machine once dubbed the "Largest Logging Engine on Earth." John Labbe collection.



Left: Climax #5, the only locomotive purchased new by Yeon & Pelton, crossing a log cribbed bridge. John Labbe collection.



Below: Two sides of this water tank are built on stumps. The pump engine on the right is a Fairbanks Morse "jack of all trades" one cylinder. John Labbe collection.



Above: Locomotive #4 with two loaded disconnected cars with long logs for use on the Panama Canal. John Labbe collection.

distance traveled by the incline fell at a steady 33 percent. He put 23,648 logs in the river during the month of October. The logs averaged about 850 board feet, and he expected to handle 100,000 board feet daily the next year. The total output was rafted to the North Pacific Lumber Company's big mill in Portland.

In early 1903, they installed a new lowering machine, built by the Willamette Iron and Steel Works of Portland. It was designed to lower three loaded cars down the incline at once, where the old machine could lower just two cars. The Willamette Iron and Steel ad in the February 1903 edition of the Columbia River and Oregon Timberman Magazine claimed the new machine as the "Largest Logging Engine on Earth." The total elapsed time for the three loads to make the round trip, including the dumping of the logs, was just twenty minutes.

As logging progressed, the narrow-gauge railroad was extended an additional six miles up the Beaver Creek Valley. In

December 1903, the company purchased their only new locomotive, a 45-ton Climax.

On September 26, 1904, the company put out 137 logs containing 142,000 board feet with a Willamette Crackjack donkey which was thought to be a record.

In December 1905, John Yeon sold the company's logging camp and 125 million board feet of timber to the Portland Lumber Company. The deal was about \$225,000.00. Equipment consisted of ten miles of railroad, four locomotives, six donkey engines, twenty-one sets of logging trucks and two lowering machines. They put in 45 million board feet of logs during 1905. During the past four years, 150 thousand board feet of logs were successfully sent down the incline. Mr. Yeon owned three fourths of the stock and was the active manager. This was the third time he sold out and planned to retire. During the next year, he devoted his attention to the construction of a new building in Portland. Known as the "Yeon Building" this struc-

ture still exists in downtown Portland. He then speculated in Portland real-estate.

John Yeon had come to Oregon in 1885 at age twenty with nothing more than \$2.50 and the clothes on his back. He turned his small fortune into millions and when he passed away in 1928 was one of Portland's wealthiest men.

Thanks to Phil Schnell for his help in preparing this article. x

# My "A" Frame Tank Locomo-



## An Unusual Kitbash

by Allen K. Littlefield  
Model photos by the author

I was impressed when I saw a model of the Mich-Cal 0-4-0T model that Les Davis built using his 3D resin printing skills. His model in On30 utilized the Bachmann Trains Forney model as the mechanism and running gear. Since I have worked closely with Les on some of my models by using his printed tanks and cabs, I thought I could do a nice freelance version of a similar, but not exact model, using the Bachmann Trains HO 4-6-0 locomotive. I previously used this mechanism and drivers for my "Scratch-Bash" article presented in the July/August 2019 GAZETTE, HO 4-6-0 to On30 0-6-0T. This is a nice running mechanism which allows slow movement, something desirable in a switching locomotive.

The Michigan-California Lumber Company shops constructed the unusual "A" frame tank, as I refer to it, on two small switch engines. The second 0-4-0T, a Vul-

can built around 1900, is the one I used as inspiration for both Les' and my models. On the prototype there were two adjoining tanks, one smaller in the rear for fuel oil and a larger tank in the front for water. On Les' printed tank there was a groove that represented this separation. I filled this groove with epoxy and then sanded it smooth to get the impression of one large water tank.

The cab is another Les Davis 3D print that I modified by adding a small wood fuel bunker. A new cab deck was fashioned from styrene and included the rear beam and coupler area. The boiler is a 3D print from Dylan Lambert at Lambert Locomotive Works out of Massachusetts. This boiler is made to fit the HO 4-6-0 motor and running gear and is the same diameter as the Forney which allows the Davis tank to fit as it was originally designed for the

Forney boiler size. Once the cab and tank were joined on the HO mechanism, various details were then added including the unique platform or "table" that held both the bell and generator. The mechanism was altered only in having the cylinders "beefed" up by adding plastic tubing over the existing ones. I described how I did this in the July/August GAZETTE.

The locomotive kit-bashes I have done have been fun. Both Davis and Lambert work in 3D printing, along with others who are now offering kits using this new method, have made the task of producing new motive power, along with rolling stock much more accessible for today's modeler. We are entering a new era of model railroad building. x

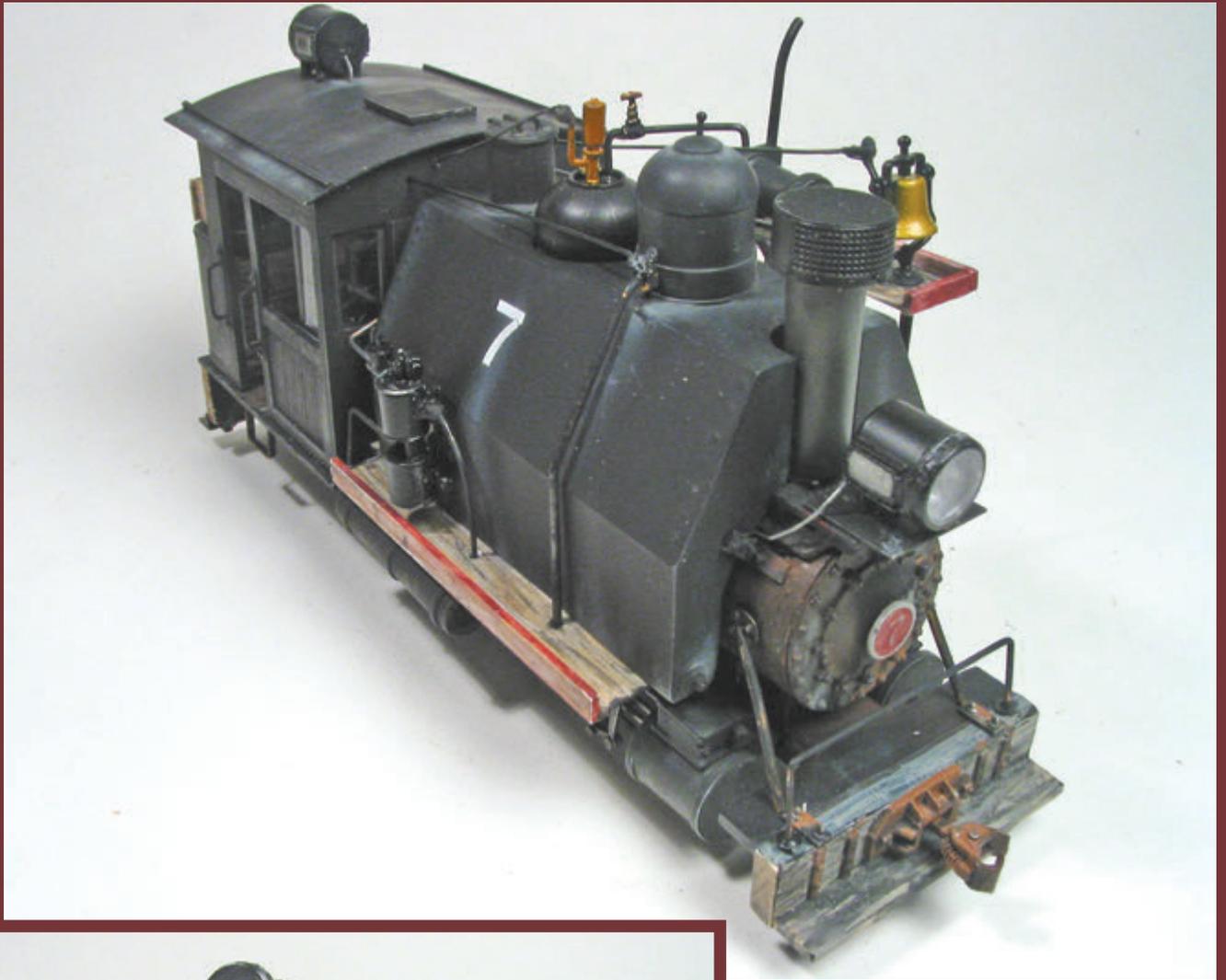
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Title photo: This Michigan-California Lumber Company 0-4-0 saddle tank served as the inspiration for the author's model. Photo by Bob Brown.

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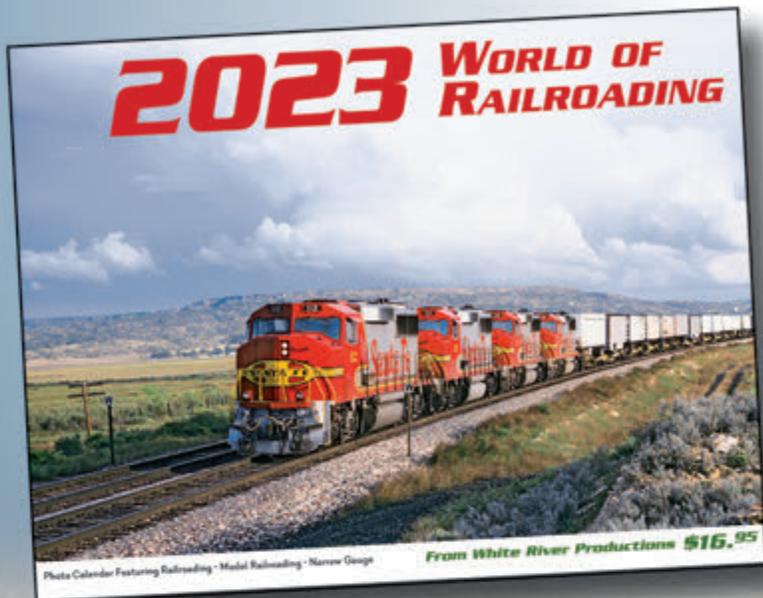
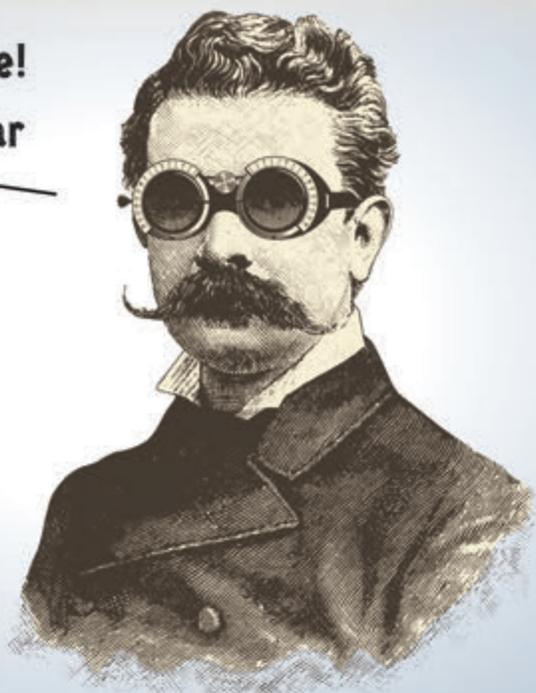


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# EXTRA 42 SOUTH



## REPLICATING THE COMMONPLACE

by Dr. Gregg Condon, MMR  
Photos by Todd Condon

My HO<sub>N3</sub> Rio Grande Southern layout occupies a dedicated train-room in our basement, and we host regular operating sessions. Much of the realism derived from an operating session stem from replicating what was commonplace. Well, okay, I violate that commonplace rule by running far more trains in a “model day” than the RGS ran in September of 1947.

What trains? My normal routine of commonplace RGS trains finds a northbound freight from Rico to Ridgway, a southbound freight from Ridgway to Rico, a freight turn between Rico and Dolores, and a passenger train each way between Ridgway and Dolores. Along with that, northbound freights longer than a certain number of cars get a helper from Vance

Junction to Lizard head. That helper job operates as a turnout of Ridgway. All this is perfectly accurate; it’s just that by 1947 my daily freight trains were mostly two- or three-day turns.

But I’ve gotta spice it up! Along with the routine trains listed above, I add an extra freight or special passenger train to each session. What extras? I currently have seven oddball extras that spend most of their time in a hidden staging yard until it’s time for them to make their special—or extra—guest appearance at an operating session. Naturally the fans go gogga and foam at the mouth just a little.

---

### STOCK TRAIN

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The stock extra was planned on my layout from the start, and my sessions almost always run one. There are nine stock cars in my car fleet. If I run all nine in a stock extra, it needs to be a three-engine train from Vance Junction to Lizard Head. More typically I make it an eight-car train which two locomotives can handle. Sometimes the stock extra will be as short as six cars if I have a crew-friendly eye to the meets I have planned. Stock extras were common on the RGS.

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### PIPE TRAIN

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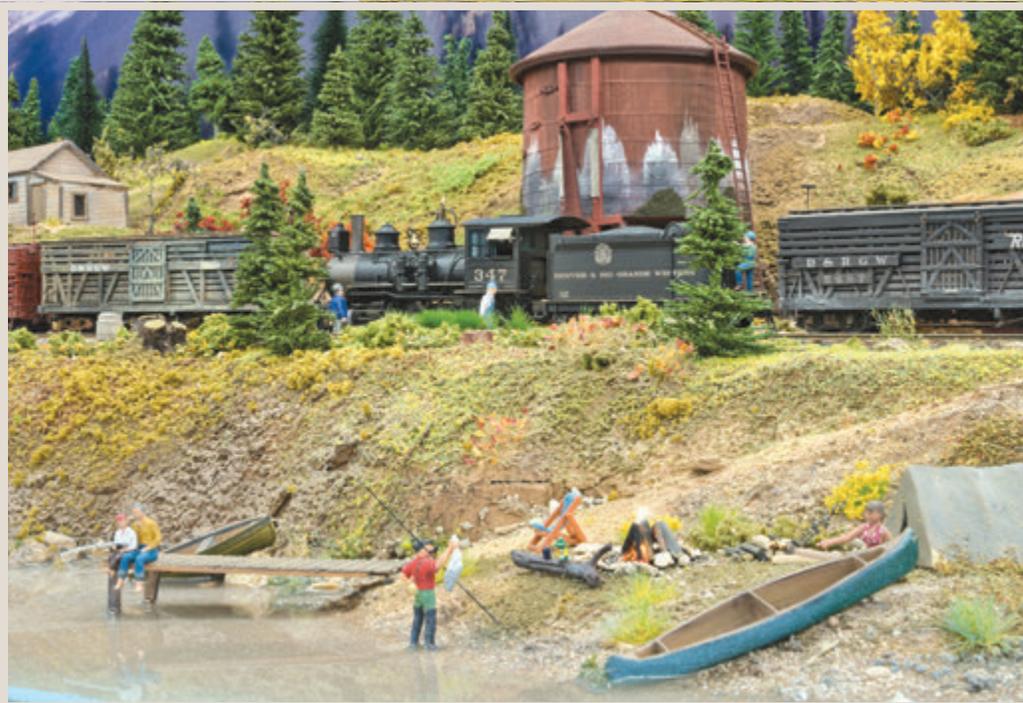
Title photo: A three-engine stock extra works up the three percent grade past Trout Lake.



Above: Who the heck in Dolores County ordered a trainload of pipe? Somebody must have, or it wouldn't be heading north on the RGS!

Right: An RGS work extra has arrived at Vance Junction.

Below: Enginemen Quist and Croonenberg prepare for #347's turn at the tank.



I have a D&RGW gondola with ends removed that is bracketed by a pair of idler flats and it hauls a load of long oil pipes. Coupled to that is a narrowed standard gauge flatcar that holds a pipe load that fits. To this four-car pipe train I add a couple boxcars that are ostensibly laden with drilling mud, or I couple up a pair of tank cars and call it an oil company extra.

## WORK TRAIN

My favorite extra is the eye-catching work train. The cars are highly modified

and thus unique. I have modified a coach, reefer, and boxcar according to photos in Morning Sun's *The RGS Story Volume XII—Locomotives and Rolling Stock*.

Flats and gondolas with their funky loads are real eye-poppers! Back in the early Seventies somebody offered a urethane casting of a C-16 tender body. Mine kicked around in my parts box for 50 years, and then I glued it to a flatcar and heaped the remaining flatcar space with parts and junk. Both the RGS and D&RGW had such cars, and they accompanied work trains to supply water and coal. My tender-on-a-flatcar shouts, "Hey, look at me!"

Another flatcar has been filled with typical work-train clutter. Guests always comment that the work train exudes real character.

## COAL TRAIN/ORE TRAIN



Above: Engine #40 crosses the first bridge in Burns Canyon with an ore extra.

Below: Deep in the canyon, the ore extra follows the Dolores River.



Coal and ore shipments in one- or two-car lots are everyday occurrences on my RGS. Sometimes, though, for sheer change of pace—and because the prototype did it—I couple half a dozen gondolas together and load them all with the same lading, either coal or ore. My long-ago article in the GAZETTE on removable carloads tells how you can make this easy, in short, build a little table that fits in the gondola with legs far inbound from the ends of the table, and glue the load on top of the table but not to the gondola. Pushing down one end of the load makes the other end pop up for easy removal.

### TIMBER EXTRA

Just prior to my layout's 1947 timeframe, the McPhee sawmill near Dolores was shipping timber to that mill from a siding called—cleverly enough—Timber Spur. That was near Coke Ovens. We model railroaders cheat with the facts all the time. I presume (dignified term for playing make believe) that this business still existed in my target year. And, since I don't have extra real estate to accommodate the Timber Spur between Burns Canyon and Coke Ovens, I have the McPhee folks shipping the logs out of Coke Ovens.



Above: Engine 42 hauls company coal over the trestles south of Lizard Head.

This extra does not need to be long to be convincing. Following fascinating prototype practice, I have the Dolores Turn out of Rico start the day by going the wrong direction out of Rico. At Coke Ovens they retrieve those log cars and then return to Rico so they can proceed on to Dolores as God intended. I have three removable log loads that I keep shuttling back and forth in this service. For such a move, a three-car train seems perfectly reasonable. I chose three cars because of considerations of track capacities at both Coke Ovens and the Dolores staging yard. Well, OK, the prototype McPhee trains were always 12 cars daily. Phooey on McPhee; what did they know about staging track capacities?

### PASSENGER SPECIAL

Anybody know why an unscheduled freight is an extra while an unscheduled passenger is a special? I don't know, either, but I conform with these common terms. While all the freight extras I've described above are the same from session to session, my passenger specials change fre-

(text continued on page 67)

Below: First job for today's Dolores Turn out of Rico is to run up to Coke Ovens to retrieve loads of McPhee timber.





Above: As the sun sets on the RGS, restored #7 and the railfan special hark back to when The Old West was young.



(text continued from page 65)

quently.

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## DIRECTOR SPECIAL

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Sometimes I run a two-car special reminiscent of the Barriger Special. Maybe the train is transporting miniature versions of Receiver Fuller and a party of legal minds, or maybe the run has been chartered by HO manifestations of my operating crew and me. In any event, the train is powered by one of my Consolidations and consists of a Rio Grande coach and my business car RICO.

---

## RAILFAN CHARTER

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On occasion I run a train that is my version of the chartered trains that were run in the postwar years of the RGS. Consists are not always the same and are powered by either a Consolidation or Mudhen and contain any combination of combine, one or two coaches, caboose, and car RICO. Someday I will add a ladder to a gondola and fill it with camera-toting fans!

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## RESTORATION SPECIAL

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Ya knows, when Blackstone offered a Consolidation in 1880's attire with diamond stack, kerosene headlight, and early paint job, I just had to have one! What to do with it on a 1947 layout? Well, I presume a steam locomotive restoration occurred decades before one happened, and I have RGS #7 restored to original appearance and booking pricey excursions. The old beauty pulls a couple coaches and the RICO. Wow, she does look nice skirting the shores of Trout Lake!

---

## CONCLUSION

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On occasion, consider putting together some unique combination of equipment. Run a special for something extra; or, um, run an extra for something special! x

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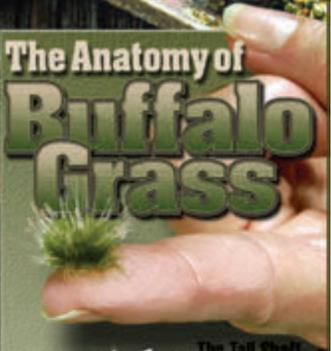
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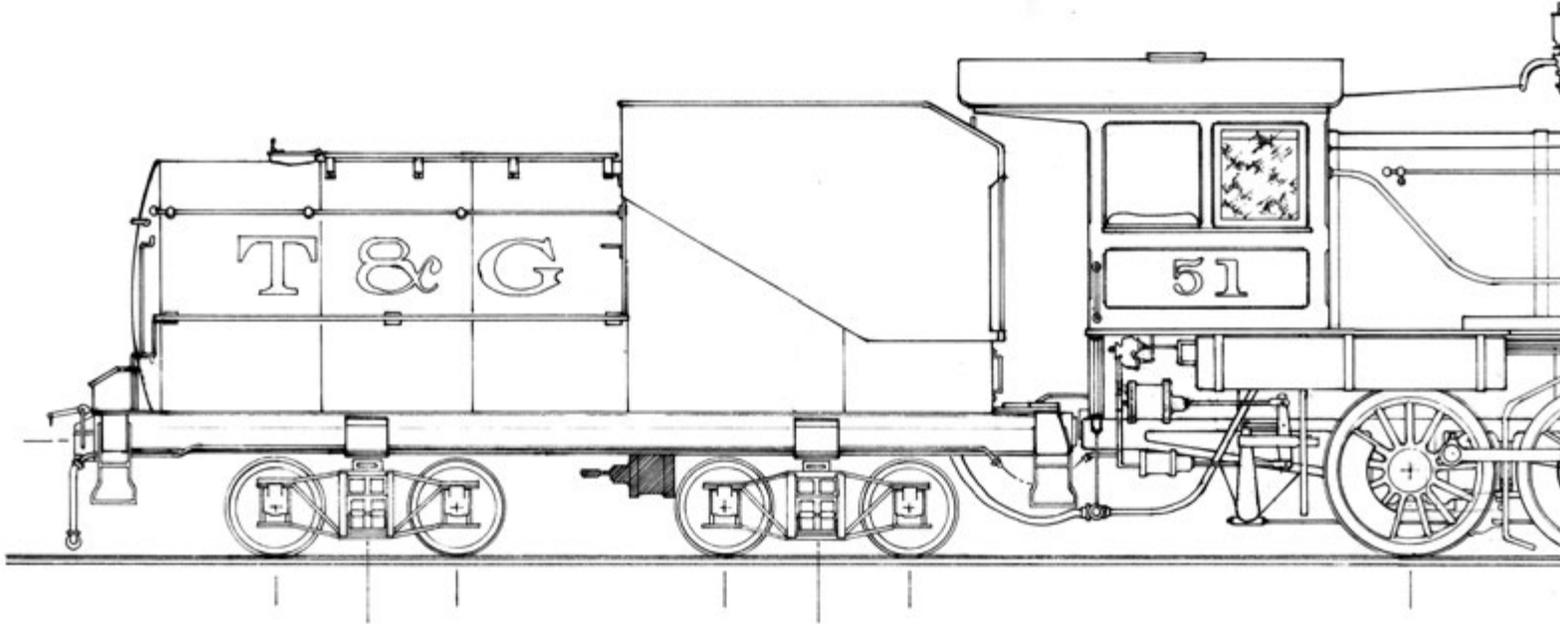
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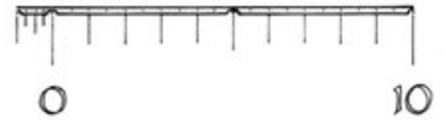
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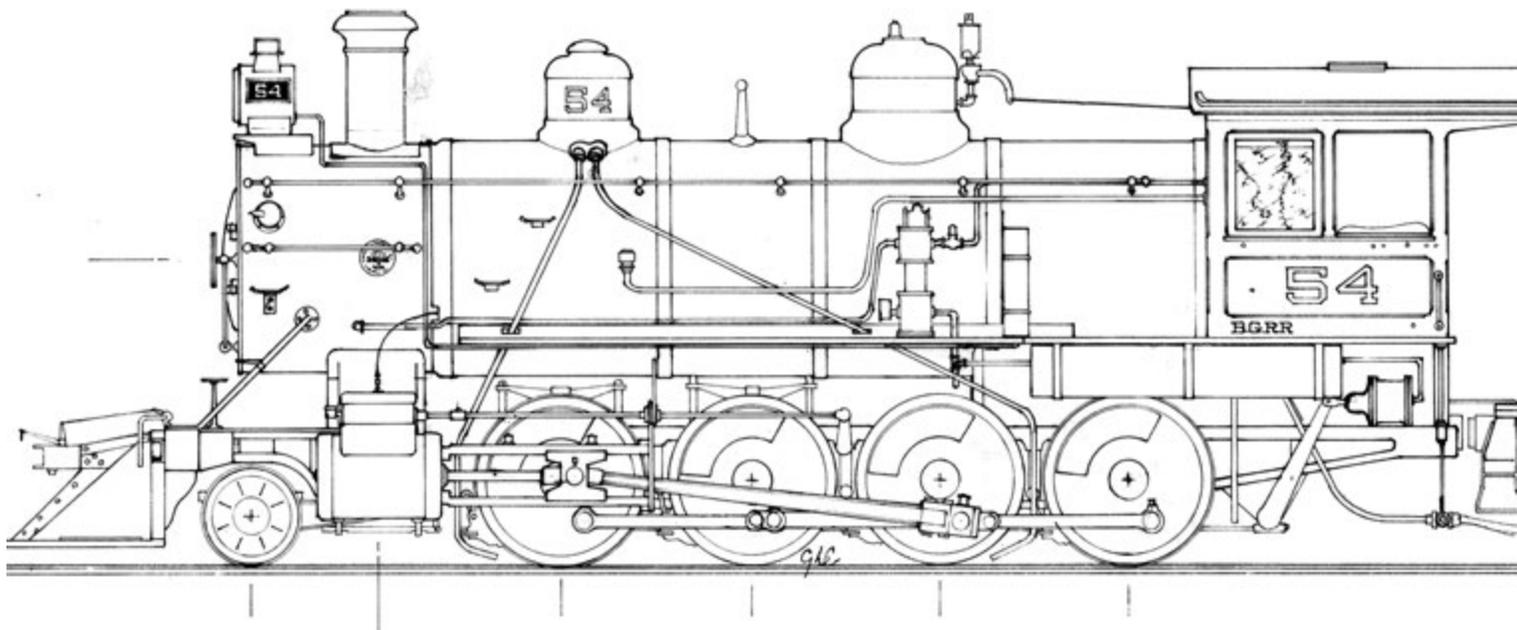


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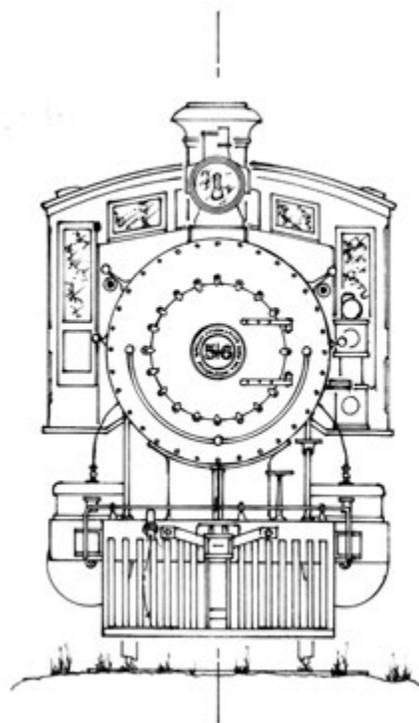
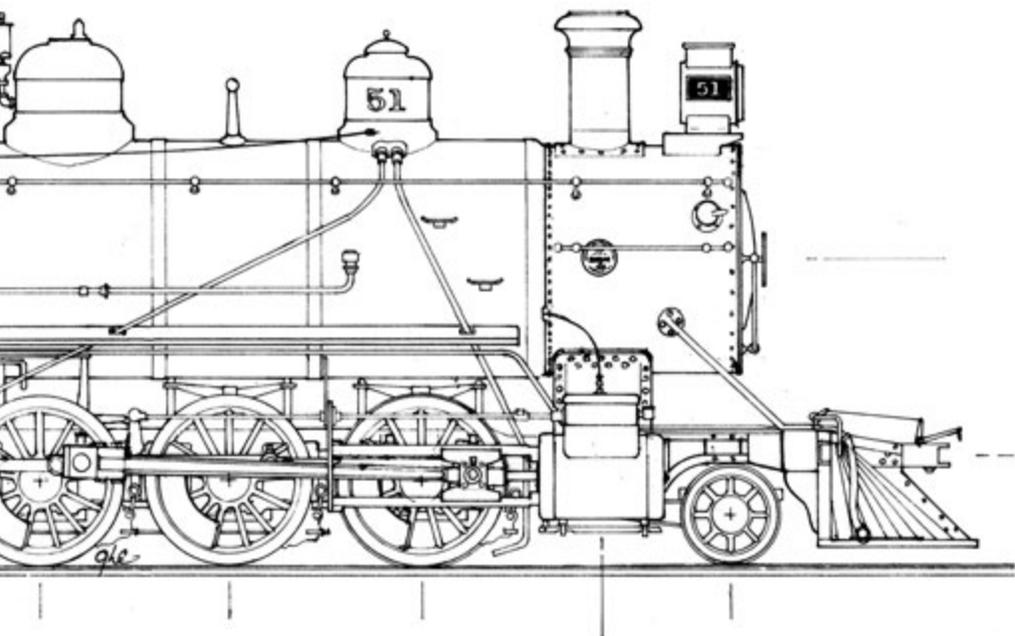


# Bullfrog Goldfield

NOS 54-55 1906

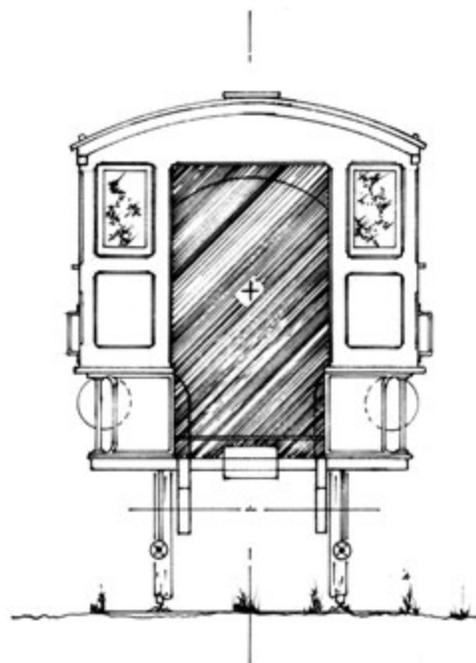
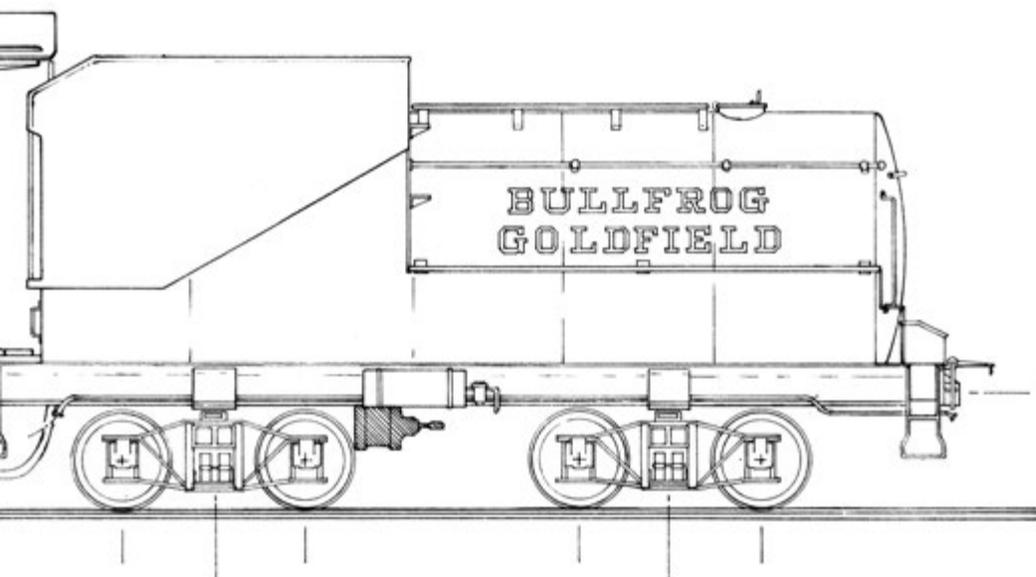


# Locomotives



## Tonopah & Goldfield

NOS 50-53      1905  
NOS 56-57      1907



# THE BROWN HOUSE

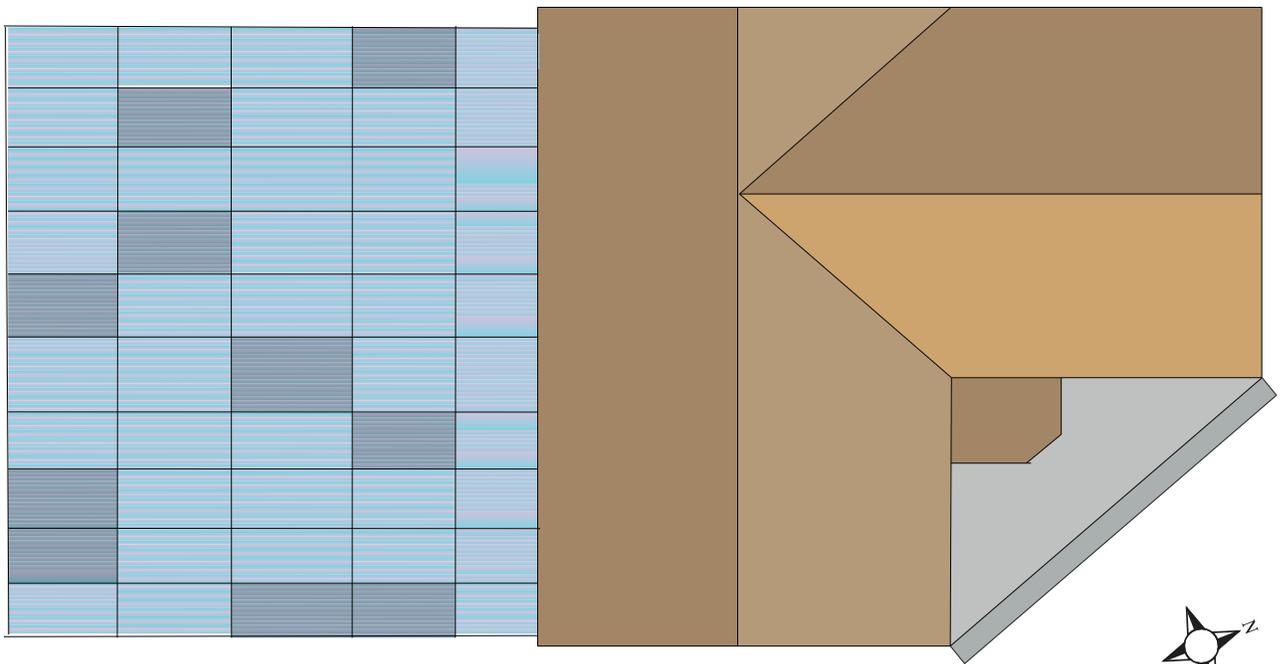
## BODIE, CALIFORNIA

DRAWN BY NEIL A. PFAFMAN  
HO SCALE

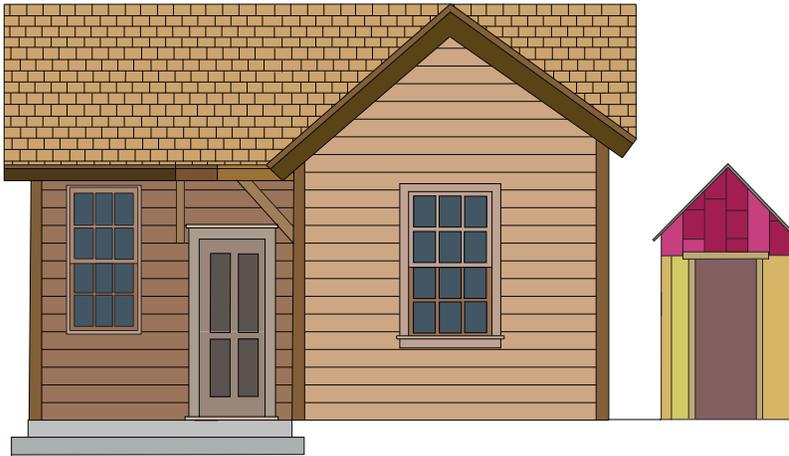
This house was once occupied by William L. Brown during the early 1930s while he was employed by the Treadwell-Yukon Mining Company.

The Treadwell-Yukon Mining Company was taken over by Red Cloud interests and new up to date equipment was installed allowing the old mine to be worked until 1931.

The house has an odd triangular cement front porch and no chimney. I suspect that when Bodie was taken over by California State Parks that repairs were made to the roof and it was easier to cover over the chimney. Also, the remaining out houses had their seats removed, holes filled in, and the doors removed.



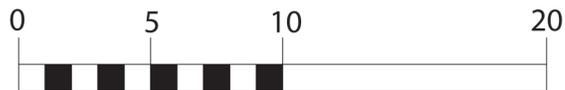
ROOF PLAN



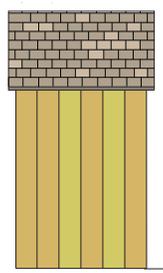
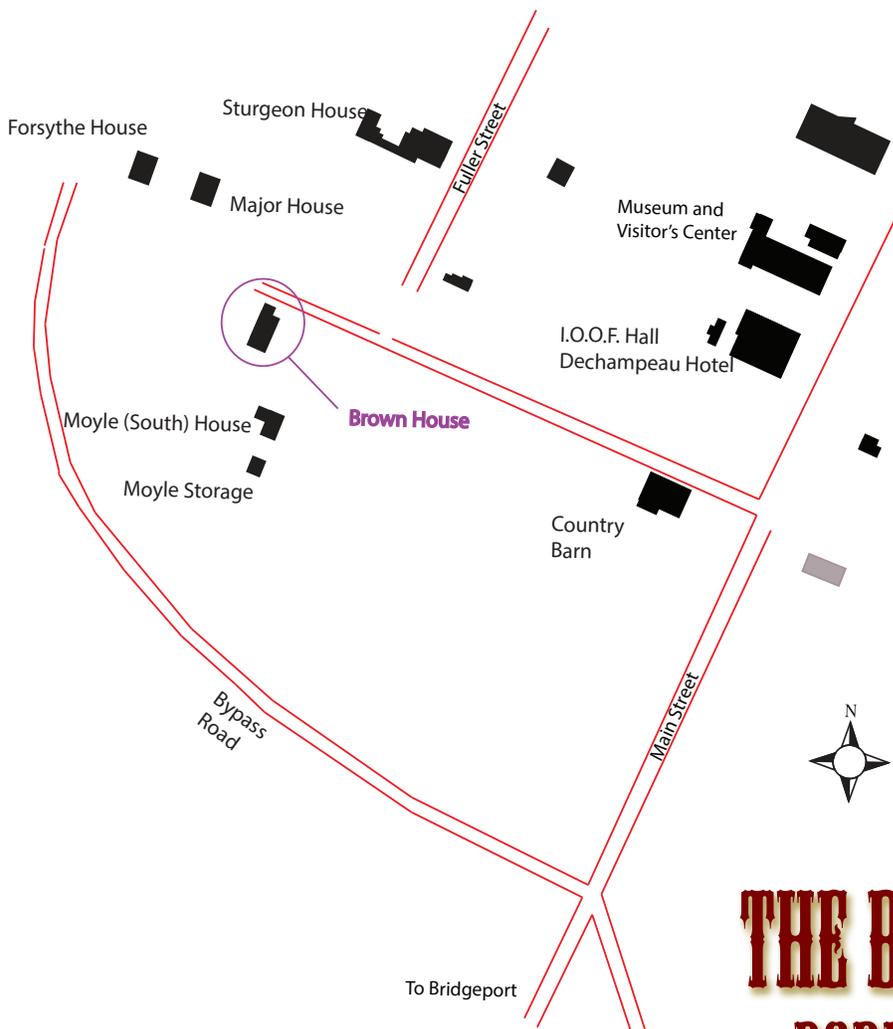
NORTH ELEVATION

# THE BROWN HOUSE ■ BODIE, CALIFORNIA

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HO SCALE



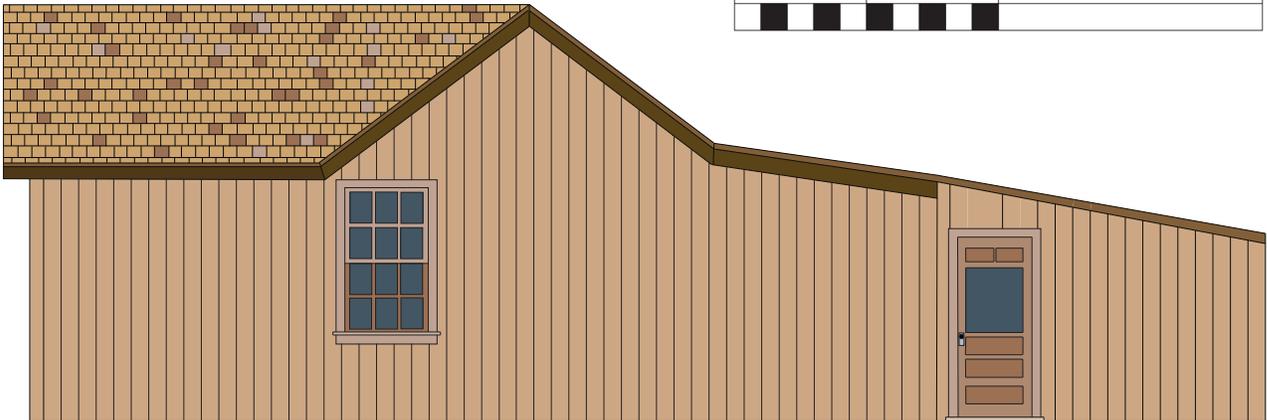
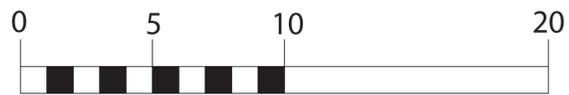
SOUTH ELEVATION



# THE BROWN HOUSE

## BODIE, CALIFORNIA

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WEST ELEVATION

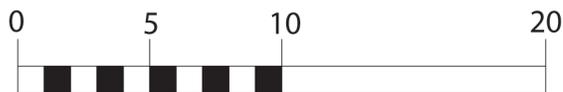
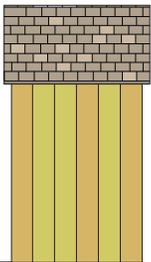


EAST ELEVATION



# THE BROWN HOUSE ■ BODIE, CALIFORNIA

DRAWN BY NEIL A. PFAFMAN  
HO SCALE



# THE FORKS CREEK EATING HOUSE on my On3 COLORADO CENTRAL & SOUTHERN



DRAWING BY HARRY BRUNK

by Dan Windolph  
Photos by the author

One of the most interesting railroad structures on the Colorado & Southern was the Forks Creek eating house. Not only was it an unusually long building, but it was built alongside a curved section of track.

This is another project inspired by Harry Brunk's GAZETTE series, "Up Clear Creek on the Narrow Gauge." Harry often wrote about the Forks Creek area during the series, and I was interested enough to build the Classic Miniatures kit of the station. I put the finished station on the unfinished Forks Creek wye section of my layout and moved on to other projects.

Much later in the series, he wrote about

the New Forks Creek Eating House, and this caught my attention. I really liked the way the buildings were joined to the station and were built in a curve to follow around the curved track. The article was Part 61 in the series and appeared in the March/April 1990 GAZETTE.

I thought it was an unusual building that would be fun to model, but I didn't think there was enough room for it on the layout, so I again went on to other projects.

I recently started working on the Forks Creek wye area again and kept thinking about the eating house. I wasn't sure I could squeeze it into the narrow space next to the station at the edge of the lay-

out. I cut pieces of paper the exact size of the space and taped them together to make a full-sized footprint of the available space. I then enlarged Harry's HO drawings to O Scale to see if the buildings would fit. I was pleasantly surprised to see they would, with a few inches to spare.

Construction was mostly straightforward except for fitting the sections together to follow the curve of the track. I was again surprised to find that the building fit the curve of my track without modification of the plan. I used NorthEastern Scale board and batten and scribed siding,

(text continued on page 79)



Above: The basic wall sections have been loosely taped together with masking tape to get a general idea of size. The thin walls are already starting to warp, showing the necessity for interior bracing.

Right: The walls have been glued together and braced, and the illustration board roof sections added.



Below: The station was already painted and weathered, but because the eating house was added later, I decided the difference in color would have been normal. The gray-painted roof was the first to receive Pan Pastel powder.





Above: Because the completed complex is so long, it was mostly assembled on my auxiliary workbench — the tops of the washer and dryer. The long strip of paper in front of the structure is the actual-sized footprint of the available space. Harry Brunk's enlarged drawing is behind the building.



Above: The roll of metallic silver plastic party decoration is at the top. Number 1 is an untouched test strip. Number 2 has been sprayed flat black. In number 3, Pan Pastel weathering has been brushed on.



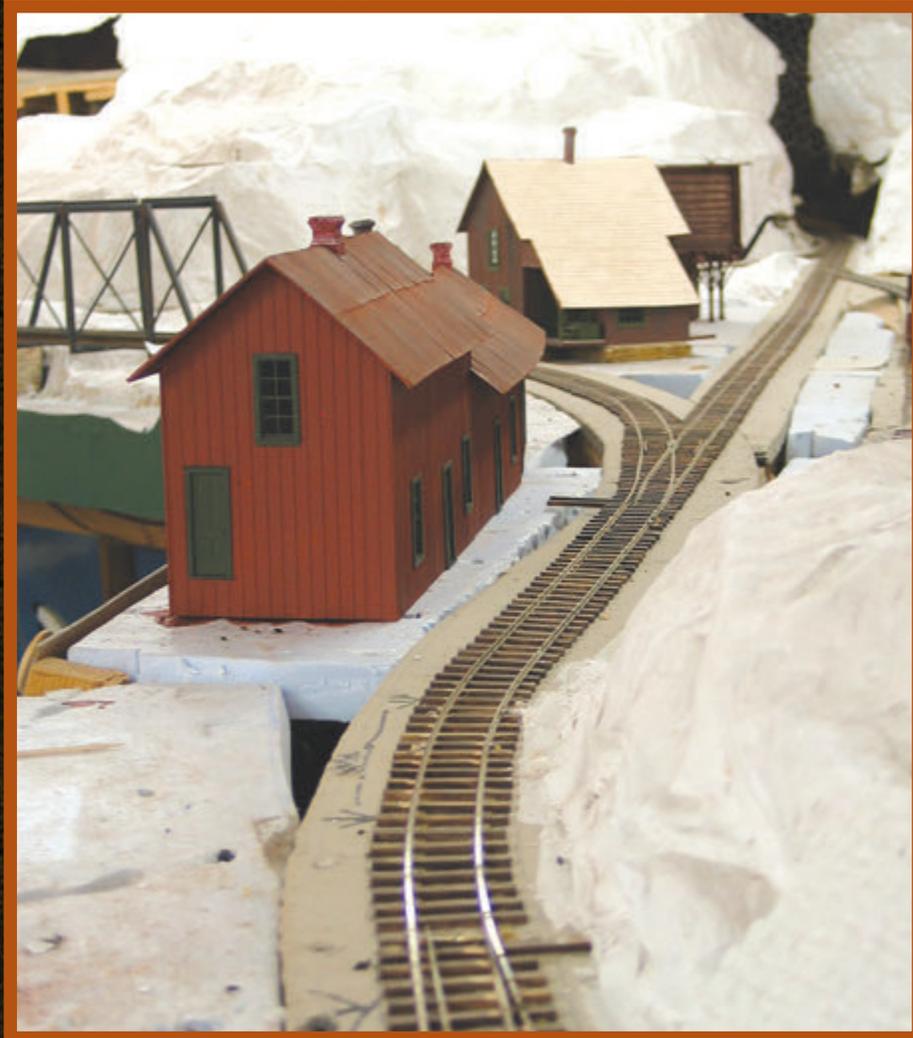
Above: The completed structure is exactly three feet long, and barely fit in my white photo backdrop.

Below: This rear view clearly shows the curve of the building.





Above: This view of the eating house roof shows the corrugated on the right side with just flat black paint, while the left side is weathered with Pan Pastels.



Right: The eating house in position on the unfinished Forks Creek wye section of my layout. The wye has a section house, water tank, pump house and steel bridge. Now that the station and eating house are finished, I can finally finish scenery at Forks Creek.

(text continued from page 77)

which was braced in the interior sections, especially where the sections were joined. Doors and windows are Grandt Line (now San Juan Models) castings, which are very close to the plans.

The various roof sections are artist's illustration board cut to fit the angled sections. At this point I had to decide how I was going to approach applying all that corrugated roofing. The station kit used paper corrugated strips, which looked okay, but I wanted something a little more realistic.

Some years ago, I found a roll of silver metallic strips that were meant to be used like crepe paper for party decorations. The material was very thin plastic, possibly some form of Mylar, grooved perfectly for O Scale corrugated roofing. However, I couldn't find a glue that would stick to it because it was so slick, so I put it in a drawer and forgot about it.

Lately, I've been using a craft glue called Aileen's Tacky Glue on a few things, so I tried it on the plastic corrugated strips. It held the test strips to the illustration board just fine, so I used it on the entire eating house roof. I painted the test strips with flat black and flat gray spray paint and decided to use flat black. The spray paint didn't stick to the slick plastic very well, with some paint collecting in the grooves of the corrugated. However, when it dried, a slightly dull silver color showed through the paint on the tops of the corrugated and I liked the effect, so I painted the entire roof that way.

I had purchased a variety of Pan Pastel colors I thought would work for weathering, but hadn't used them very much. I tried them on a few painted corrugated test strips and liked the results. I wanted the look of a normally weathered roof rather than a decrepit one. I applied the

powder with small brushes rather than the soft sponges the manufacturer recommends.

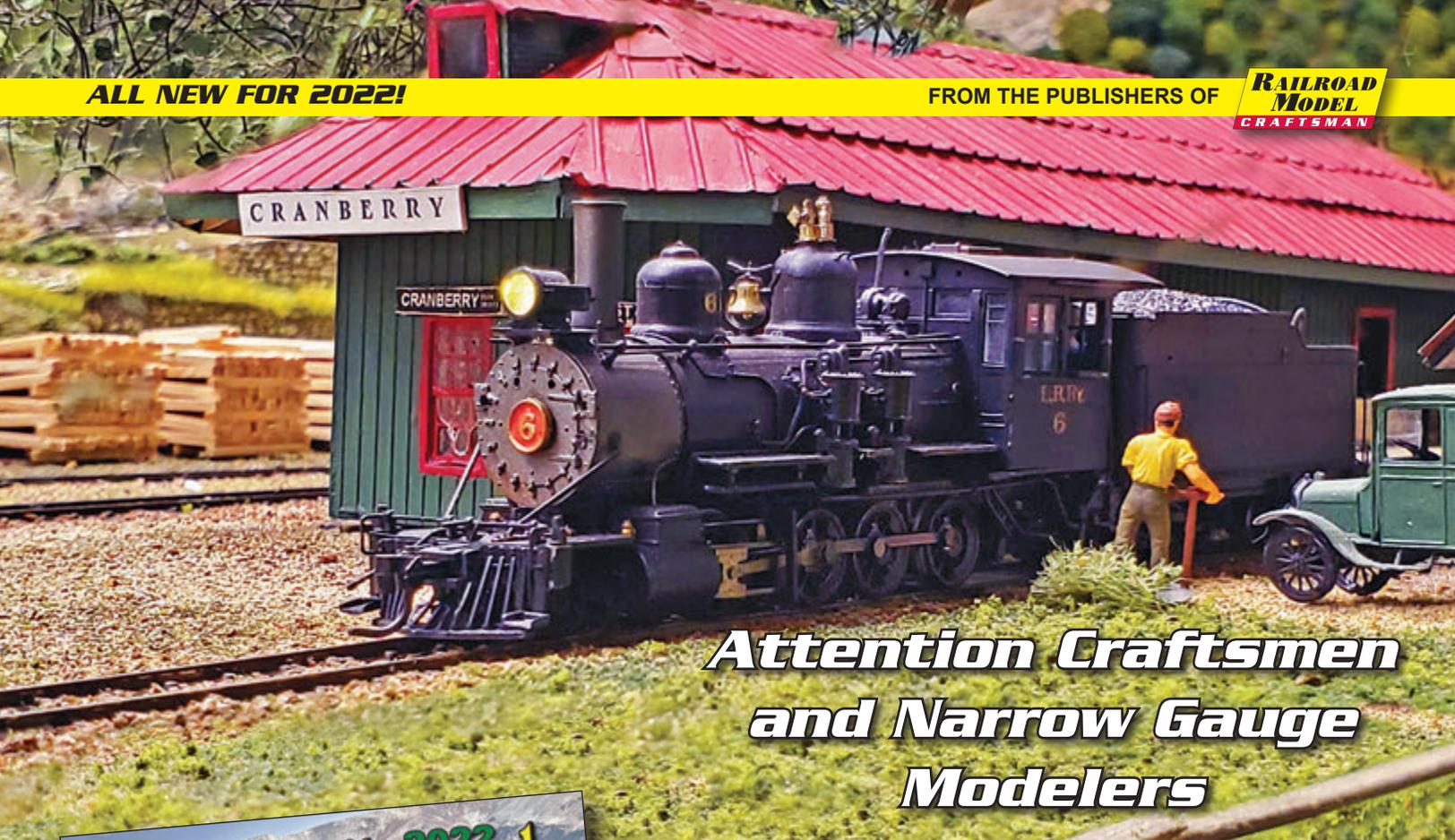
The location of the eating house and station on my layout is certainly not ideal, but finally getting it built should be an incentive to finish scenery in the wye area. More importantly, building the very long, curved structure was challenging, but fun. It also reminded me that, at three actual feet long, just how big O scale buildings can be.

Next time, we'll take the train from Forks Creek to Georgetown to look at the Alpine Hose #2 firehouse. x

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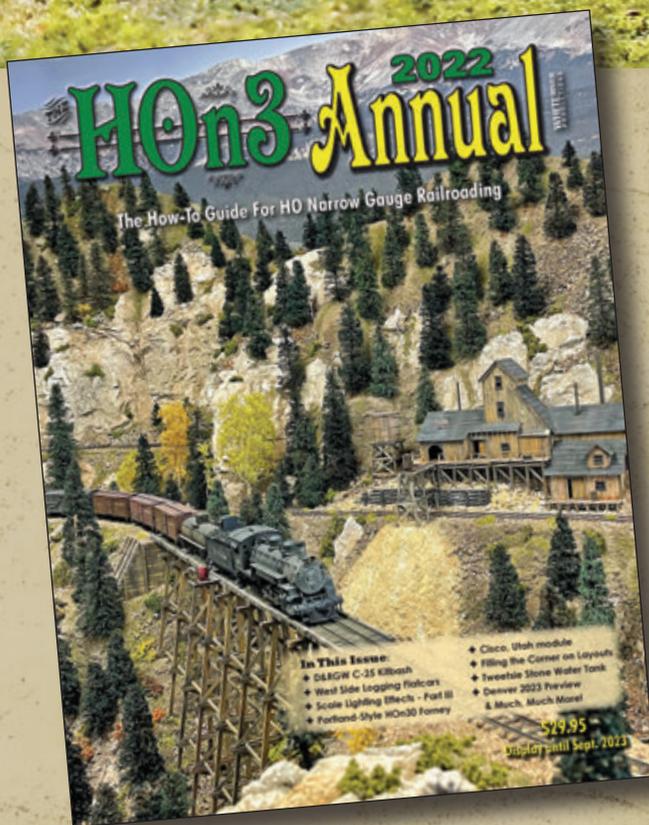


## Attention Craftsmen and Narrow Gauge Modelers

Geoff Skunkard photo

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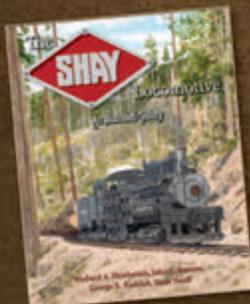
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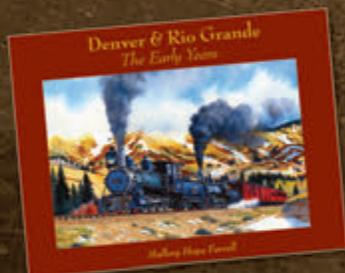
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# OLD PLACERVILLE PETROLEUM DISTRIBUTORS

## PART 3: TANKS AND COMPLETING THE SCENE

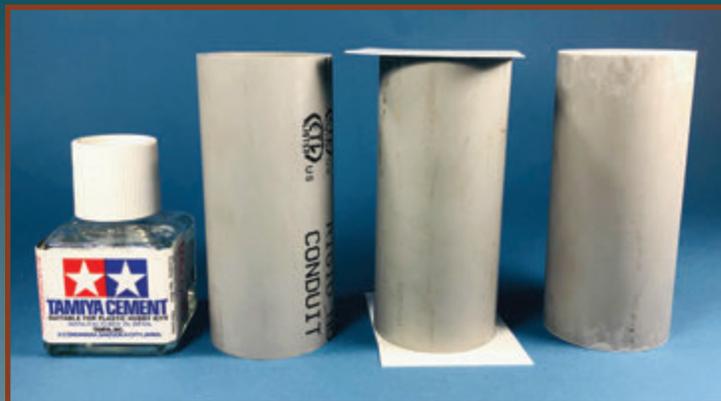
by Craig Symington, MMR  
Photos by the author

In the previous two issues of the GAZETTE I described how I built HO models of the Conoco and Texaco petroleum distributors at Old Placerville, Colorado. I'll continue the story by describing how I built the tanks for both distributors and how I blended the structures into my layout. Please follow along with the photos and see how I accomplished this.

---

Title photo: I built the entire petroleum spur at my workbench, including the scenery. This diorama replaced an existing spur on my layout and was blended into the basic scenery. With the Conoco distributor in place, I now have a destination for my San Juan Models tank cars. The Texaco dealership provides me with a destination for my CYCX numbered narrow frame tanks as well as my double dome TCX tanks. The people of Placerville and the adjoining Paradox Valley will be well served for their petroleum needs.

Right: Initially I tried making the tanks using a styrene sheet wrapped around styrene disks, but all I managed to accomplish was to create a badly dented tank that looked like it was being crushed in a junk yard. That failure sent me back to the drawing board. After some thought, I took a trip to the local home improvement store and bought a length of 1¼-inch diameter plastic conduit. The outer diameter worked out almost perfectly to the 12 feet indicated on the Mike Blazek drawings ([www.blazekplan.com](http://www.blazekplan.com)) that I was using as a guide. I cut five pieces of conduit to 27 scale feet in length, then capped the ends with .020-inch styrene. I used Tamiya cement which seemed to work just fine with the plastic that the conduit was made from. The caps were trimmed to size and filed to create a smooth union. This photo illustrates the steps involved.

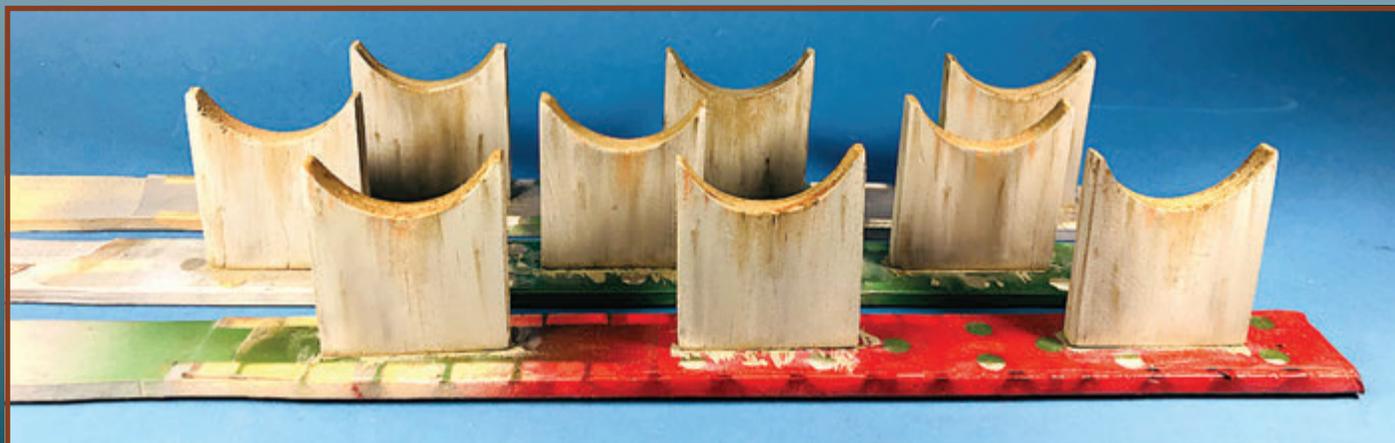


Above: Most prototype tanks constructed in the early 1900s were made by riveting a number of rolled steel sheets together. To simulate the center section of sheets I wrapped a 13½-scale-foot wide piece of .010-inch styrene around the tanks. More Tamiya glue and rubber bands held the styrene in place.

Below: The three sets of Texaco tank saddles were painted a base coat of Floquil Concrete. To create texture and make the wood blocks look more like concrete, I rubbed on three shades of gray Pan Pastels that ranged from an almost white tone to a medium gray. I then used an earth-colored Pan Pastel and various rust colors to add streaks to the concrete saddles. When I was satisfied with the look, I sprayed them with Testors Dullcote.



Above: The three Texaco tanks sat on top of poured concrete saddles. I used 5/16-inch plywood to simulate these saddles. The first step was to cut the plywood into strips that were a scale 12-feet wide. I then drilled 1 5/8-inch holes about three real inches apart. Then I cut through the middle of each hole creating a bunch of blocks of wood with a half a circle cut out of each end. These were then used to create saddles ten scale feet in height. When I was done, I had a pile of concrete saddle forms. I selected the best-looking ones and grouped them in sets of three saddles for each of the three Texaco tanks. The wood was then sanded smooth, and I ended up with basic forms that were used to simulate the real concrete saddles.



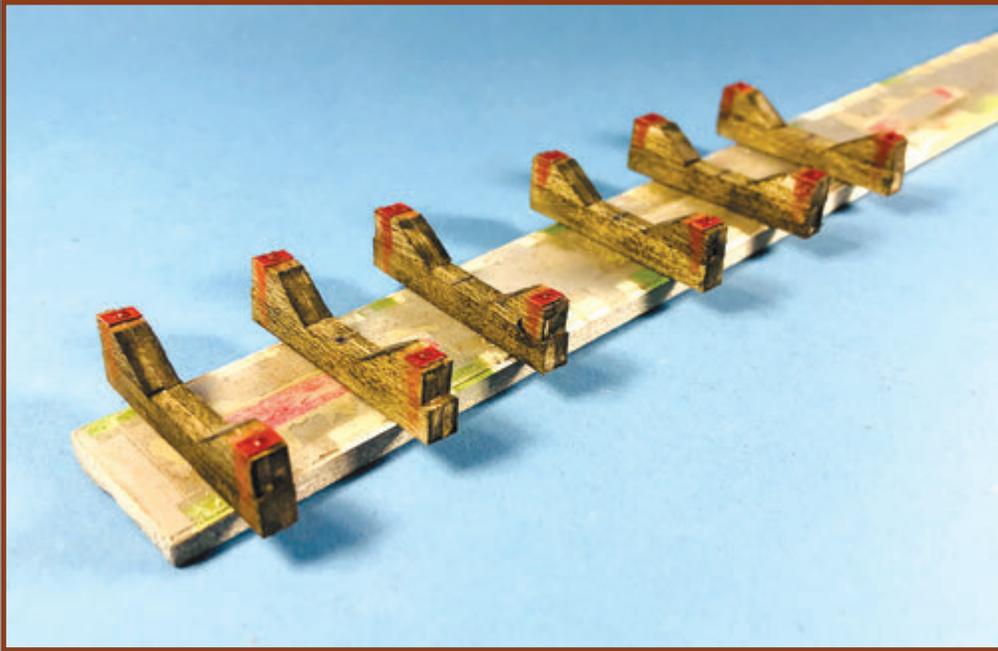


Left: With the Texaco tank saddles sorted out, I focused on finishing the tanks. I felt they needed more detail, so I used Archer textured rivet decals (AR 88098 & AR 88031) to simulate the seams in the tank pieces. I also added a row on the tank ends to simulate two pieces of metal making up the ends. These rivet decals were tricky to apply to a cylinder until I used some left-over tank saddles taped to a plastic box as a cradle. This setup made it much easier to apply the decals.

Below: I made the Texaco and Conoco tanks identical. There is one photo in the RGS Story Vol. 1 (Sundance Publications) that shows the Conoco tanks in the background, and they appear to be about the same size as the Texaco tanks. This made construction a lot easier. With all the Archer decals applied, I had all five tanks ready for paint. I ended up painting them Floquil Reefer White shortly after this photo was taken.

Below: In the RGS Story Vol. 1, if you look closely at the photos of the left-over tank bases for the Conoco facility, you'll see tapered concrete pedestals. To create these pedestals, I used my table saw to cut basswood 3-scale-feet square. I cut this wood into 10-scale-foot lengths. Placing a 2-foot-square mask over the end of each pedestal blank, I traced a 6-scale-inch outline on the perimeter of the end of the wood blocks. Using a belt sander clamped in my workbench vice, I sanded the four sides to a taper using the 6-inch mask as my guide for making one end 2-feet square while leaving the other end 3-feet square. When I finished making these pedestal blanks, I painted and weathered them using the same technique as I did for the Texaco tank saddles.





Left: The prototype photos only show the Conoco concrete tank pedestals but don't show any details of what the tanks sat on. Since there is no sign of steel bracing in the photos, I felt that there was probably some sort of wooden frame between the pedestals and the tank. Using 8- x 16-inch scale lumber, I created large cradles like what you'd see on a Conoco tank car. These cradles were stained with Hunterline Driftwood and Medium Brown stain. On the prototype, the cradles would have needed to be fastened to the pedestals. To simulate that, I made metal plates using 1- x 10-inch styrene with Grandt Line #5123 NBW castings to make it appear like the cradles are lag bolted to the tops of the concrete pedestals. I weathered these cradles with rust colored Pan Pastels.

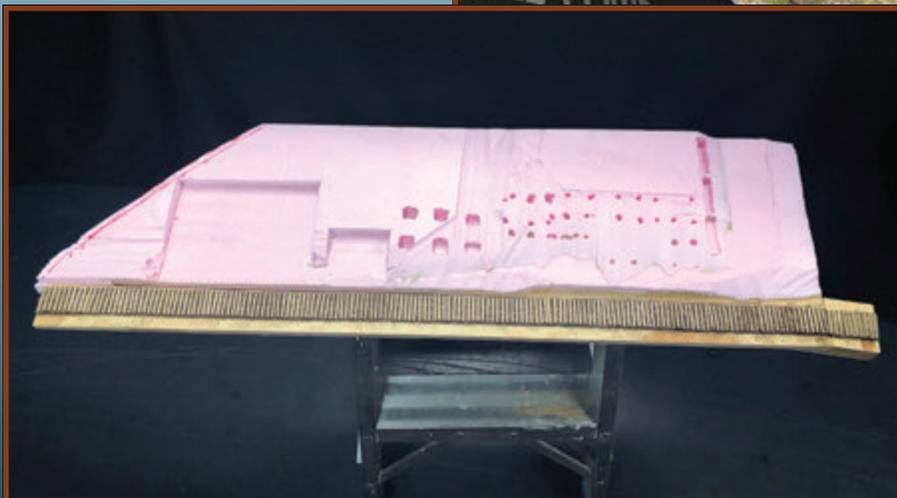
Below: With the basic structures built, I wanted to see how they would fit on the existing Old Placerville spur on my layout. I didn't bother with the tanks, but set the other structures in place while leaving room for the tanks. I wanted to get a sense for how everything would fit into the scene, verify the elevations, and see if I could utilize the existing spur trackage. It was apparent that space was going to be tight, and the spur would need to be moved to make room for all the buildings.





Above: I cut a piece of plywood roughly the size of the area where all the buildings would fit while leaving room for the spur track. Installing the spur track at the workbench would be the way to go. This part of my layout is so deep that I can't reach the very back of the scene, let alone hand lay track back there. In this photo, I'm using the meter stick to visualize the center line of the spur track. The wooden block in the photo has a 24-inch curve cut into it. I used this gauge to plan my layout long before the advent of "sweeps" for simulating radii. Nearly all my curves have a 24-inch radius or greater curve. With the gauge and ruler in place, I could see where I needed to re-construct the scenery and track.

Below: With the general position of where the petroleum distributors would fit into my Old Placerville scene decided, I did one more re-check of the scene with the buildings in place. I also used pieces of flextrack to determine where the spur would be aligned. Once the buildings were in place, it became apparent that I didn't have enough room for the Texaco tanks. On the prototype, they were further to the right and not blocking the end of the building. If I moved them in the scene, I wouldn't have room for the highway that ran behind the distributors and my petroleum customers wouldn't be able to access these distributors. Looking back at the prototype photos I could see that at least one of the Texaco tanks was removed and crumbling concrete saddles were left in its place. I decided that a fair compromise would be to have only two Texaco tanks and replace the third with crumbling tank saddles in front of the building. Using some left-over saddles, I cut them up to make them look like they were crumbling away and painted them to match the others. This helped control the depth of the scene and left room for the highway.



Left: I used 1/2-inch foam to create contours under and around the structures and tanks and used a foam hot cutter to contour the land, cut in the areas where the buildings were set into the hillside and add holes for the foundations to find level ground. You can also see that I've added ties for the future spur track.

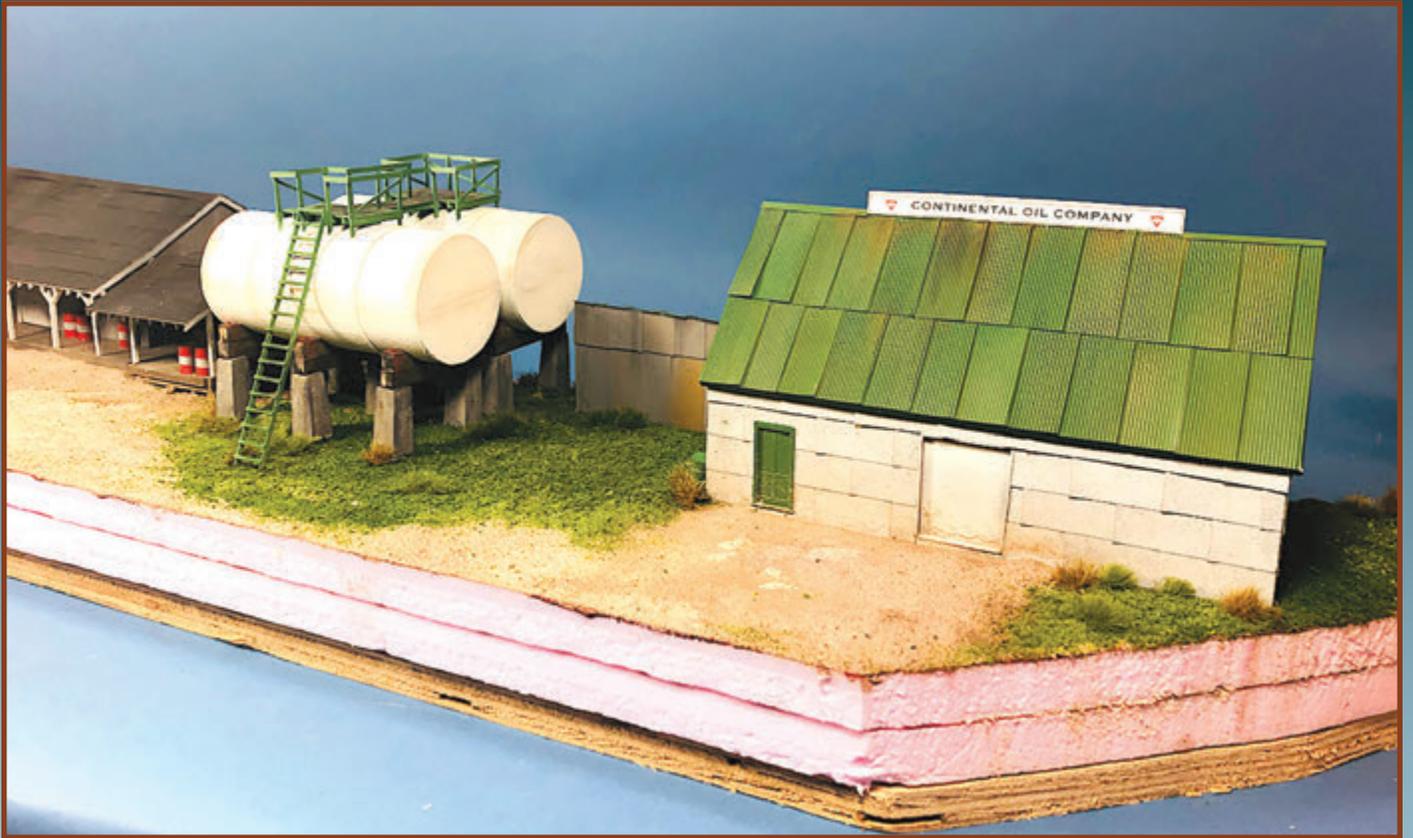
Right: I did one last fitting before I started on the final scenery. The old scenery has been removed in this photo and supports (not seen) for the petroleum distributor scene have been installed in the bench work. All the buildings are set in place and concrete ruins replace one of the Texaco tanks. I've also spiked rail along the spur. The sub roadbed and ties have also been installed on the layout and the bridge has been relocated.



Left: I added sanded grout and ground foam to the scenery base after setting the buildings into their final position. Once the tanks were fully glued to the diorama base and being careful not to get glue on the tanks, I built the tank walkways in place. These walkways and ladders were built using various sizes of Evergreen Styrene.

Right: This is the trackside view of the Texaco distributor set into the diorama base. The tank walkways have been built out of styrene, painted Floquil Reefer White and set in place. Precision Scale #2935 Valves and .020-inch wire have been added to the ends.

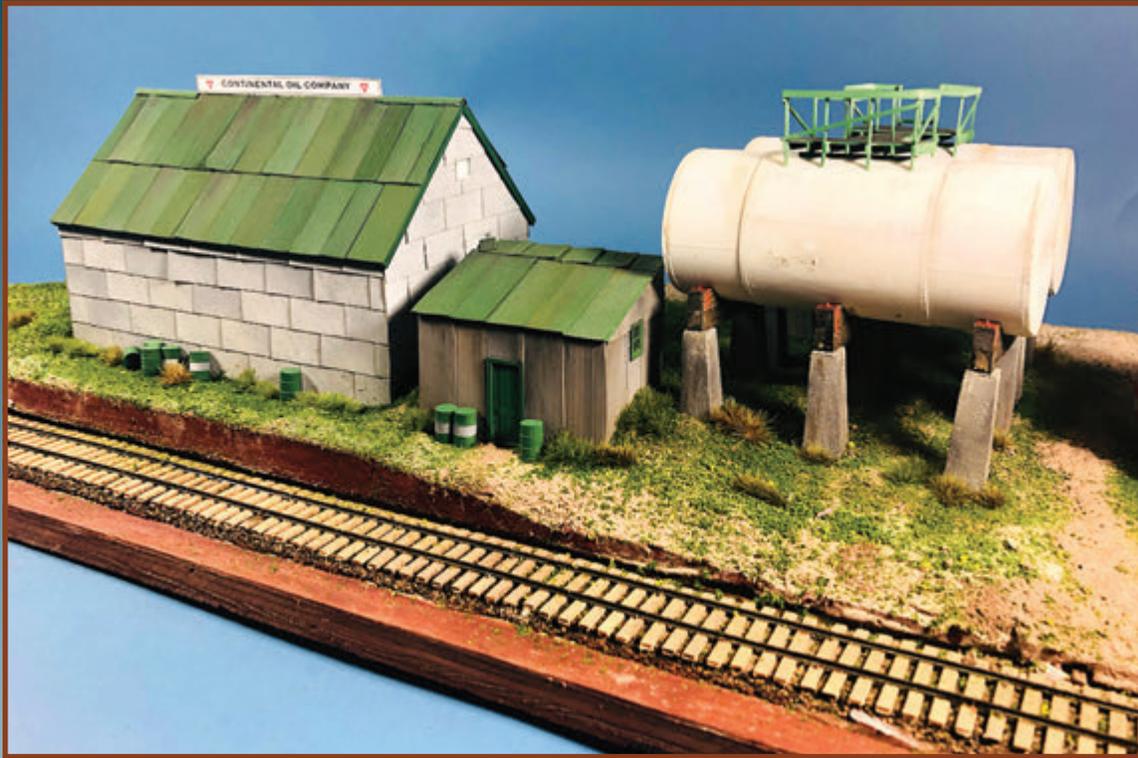




Above: This is the highway side view of the Conoco distributor. The buildings have been set into the scenery and the tank walkways have been installed. The walkways were painted Humbrol #78 Green and weathered with Pan Pastels.

Below: This is the completed Texaco dealership as seen from the highway. Unfortunately, all this detail will rarely be seen on my layout since it faces the backdrop.





Left: This trackside view of the Conoco distributor has been scened on my workbench and is ready to install. I didn't bother adding ballast to the spur since it will be on a grade on my layout but sits flat on my workbench.

Below: The scenery is roughed in, and this project is completed for now. In the future, I'll add more details and scenery. x





by Charlie Getz

Photos courtesy Stan and Sheldon Schwedler and Oconto History

## CORONADO SCALE MODELS—PIONEERS

If you have copies of Bob's first magazine *Finelines* you will find Coronado Scale Models first ad in the September 1966 issue. If you have the very first *GAZETTE*, March/April 1975, you will find an ad from Coronado Scale Models. If you look through this issue of the *GAZETTE*, you will also find an ad for Coronado. Indeed, you will find an ad from them in every issue. Probably the best-known supplier of narrow gauge products, Coronado is also the "last man standing" of exclusively model railroad hobby shops in the immediate Phoenix area. (At least they were my last trip there in 2021.) Coronado has been operated by Stan and Sheldon Schwedler since its beginning and it seems only appropriate to continue my occasional discussion of pioneer or significant narrow gauge suppliers with Coronado.

In the digital era of the internet, Coronado is analog. They not only preceded the internet, but they also anticipated it by marketing through mail order service to narrow gauge modelers around the world from the very beginning. By not just stocking and supplying products, but also offering repairs, modifications and proprietary products; Coronado has been the "go to" place for narrow gauge for decades. Stan and Sheldon Schwedler are synonymous with excellent service and helpful information. Often mistakenly identified as "twins," the brothers are separated by five years. When confronted by this fact, they reply that they are still twins; their mother just experienced a difficult delivery!) It is not surprising that they ended up in the

model business which was probably in their genes.

Their parents, Chester and Eleanor Schwedler, lived in Oconto, Wisconsin, north of Green Bay, when the children were born. Sister Sharon came first in 1937 followed by Sheldon in 1940 and Stanley in 1945. (The Schwedlers ruefully note their parents' obsession with "S" names.) In 1946, Chester started a toy company in Oconto, naming it after his youngest child. The Stanley Toy Company occupied a venerable brick building at 1301 Main Street on the corner with Park Avenue. The building started as a Music Hall, later a woolen mill and finally, Schumacher's Grocery & Meat Market. This factory must have been a significant occasion as the local paper carried articles about the proposed toy factory use, its grand opening and even an automobile accident when a wayward car slammed into a corner of the factory. Stanley Toy Company manufactured cast iron toys, like those of the turn of the 20th century. But they were made of aluminum. They specialized in horse-drawn wagons, carriages, and Easter Bunny carts. A picture, courtesy of Oconto History, shows some of their products which included a steam pumper fire engine and ladder truck. All came painted. One newspaper story mentions Stanley Toy exhibiting at the prestigious New York Toy Show. Another describes a tour given at a local Kiwanis club in 1948, including Stanley's new foundry on Pierce Street and noted that sales were increasing. Indeed, Chester Schwedler cited that after only two years

in business, there were sales in all 48 states and that Stanley Toys were sold in every Montgomery Ward store. By the way, each touring Kiwanian was given a toy of their choice at the conclusion of the tour.

Tragically, the Stanley Toy Company abruptly ended with a catastrophic fire on December 29, 1949. Although only in business for a few years, its impact was measurable. Stanley Toys are still sold on eBay and other sites, and in 2012, David L. Retzlaff, who owns what is left of the building, published a 75-page book, *The Stanley Toy Company: Fame to Fire*, which commands top prices on the used book market. In 1951, the family moved to Phoenix as Chester was offered a job in sales for a company in central Phoenix. A year later, their mother, Eleanor, died leaving the raising of the two boys to 14-year-old Sharon. She continued doing so through high school and beyond, eventually earning a degree in education. She later married the next-door neighbor, Donald Caughlin, who joined the Air Force. After multiple transfers around the country and two sons, they ended up in North Carolina, where they lived for many years. Sharon outlived her husband Donald (2006) and youngest son Larry (1981) and moved to San Antonio to be cared for by her eldest son Bill, where she died in 2015, ironically in a hospice called the "Coronado."

When Sheldon finished school, he worked as an architectural model builder for a local Phoenix firm. Stanley later did the same. In 1965, Coronado Scale Models was created with Sheldon as "owner"



Above: A selection of Stanley Toy Company cast aluminum toys. Photo courtesy Oconto History.

Below: A quarter size gas turbine built by the Schwedlers for photographic purposes.

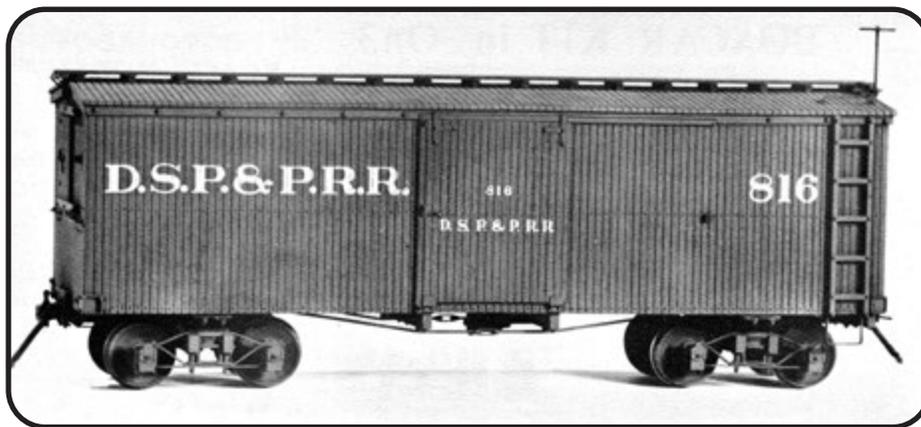


and Stanley as sole employee, a relationship which continues to the present. They picked the name after the first narrow gauge engine of the Coronado Copper Company, described in Volume III of David Myrick's epic Railroads of Arizona series. Sheldon started with Lionel and then switched to O scale. Dick Andrews and his Railroad Model Craftsman Narrow Gauge Junction articles, plus a visit in 1955 to the Knott's Berry Farm model shop, cemented narrow gauge modeling in Sheldon's mind.

For a few years, the industrial model business was lucrative as Phoenix began its steady increase in population. There was a continuing demand for architectural models used to obtain permits or sell real estate. Motorola was an increasing source of business as an employee in their art department fed work to the brothers such as making large models of transistors and power units, award plaques and even cabinets. The models were used for ads and product listings in catalogs. The Schwedler brothers were also employed



Above: Architectural scale model of the Heart Rehab Fitness Center designed by Richard Dorman.



Left: The pilot model for the On3 DSP&P boxcar kit produced by the Schwedler brothers and their friends.

by noted architect and railroad author Dick Dorman on some projects of his, as well as for other major firms. Their model work changed from primarily housing models to hospitals, convention centers and the occasional 3D topographic model of a subdivision with Monopoly style houses. There were some impressive projects. They made a huge topographical map for Kennecott Copper, and one of the Del Webb golf courses. The latter showed individual holes with elevations to facilitate landscaping planning. Over time, architectural models became more demanding and expensive to produce so

the demand began to dwindle.

The Schwedlers began to pivot in the early 1960s from purely industrial modeling. In 1963, they met Bob Brown and Valley Car Work's owner Lee Klaus in Bakersfield at a PCR meet. In 1965, Sheldon bought the mail order business portion of Valley Car Works from Lee and in 1966, the inventory. The Schwedler's also began selling plans from Herman Darr, Ken Pruett, Lee Klaus, and David Braun. In the early 1960s, Cliff Grandt began his line of parts, a natural fit to the Valley Car Works line, and the Schwedlers soon added that line to the products available by mail order. The 1966

PCR convention in Riverside, California, was the first time Coronado displayed their wares. They have since been a reliable vendor at many narrow gauge, NMRA and O Scale West trade shows.

From 1965 to 1970, the Schwedlers had occupied part of a building on East Cypress Street in the Coronado District of Phoenix. Next door was a retail operation. The Schwedlers decided they needed a retail store and fortunately that space became available. This enabled them to continue their model making activities and also operate a retail outlet for their growing line. In 1970, Coronado Scale Models opened

the doors at 1544 E. Cypress Street, where they are located today. The hours are interesting: 8-12 then 1-5. Although not a Union shop, they religiously take an hour for lunch. Now anyone who has visited also notes two things: there is no large "Coronado" sign, and the neighborhood is, how do you say .... a bit "iffy"? The Schwedlers report it used to be worse. One day, the brothers watched a "sting" operation, listening on the police scanner, as an undercover "hooker" stood in front of their store attracting "Johns" who were promptly arrested. No, the "Johns" were not Coronado customers. But yes, Coronado is a full-service hobby shop! Given the neighborhood, which by the way is safe during the day at least, no large sign is needed. This is not a shop for walk in customers but rather a destination shop. And there is never a parking problem.

What sets Coronado apart, however, is not as the source of narrow gauge kits and parts, though it is. Coronado is renowned for the advice, service and unique products offered for narrow gauge modelers. On internet narrow gauge chat rooms, whenever anyone has a question about how to convert an On30 locomotive to On3 or add details, the response often is to contact Coronado for help. Stan and Sheldon are famous for performing miracles and dispensing sage advice to solve a myriad of modeling problems. It only makes sense given their professional backgrounds.

In 1970, they took over an On3 DSP&P boxcar project from Jon MacNeil of Denver and manufactured 100 kits. Ron Parkhurst (Timberline) cut the wood; Allen Brewster drew the plans. Charles Brommer did patterns (as did the Schwedlers). Brommer also made couplers, bolsters, and brake cylinders. Cliff Grandt provided ladders and other detail parts, and Chuck Francis of Thin Film did the decals. A stellar cast. This kit was sold as a body-only kit. The listing of sources demonstrates the level of care and accuracy put into this kit. Also offered were castings for a Gilpin truck, done before Grandt Line's Gilpin ore car kit. A resin cast square water tank for an Uintah water car was proposed but never produced because of casting problems. It is my understanding that Coronado also offered conversion services and other modeling assistance that made narrow gauge modeling easier to accomplish in the days before the plethora of products later available.

In the 1960s Stan and Sheldon, along with David and Sondra Gast, and Donn Barber founded the "30" Gauge Guild. An On2½ (as it was then known) group who

built a small On30 layout and wrote it up for the November 1962 RMC.

Having started in HOn3 narrow gauge modeling in 1969, I can attest to the challenges faced when the only ready-to-run motive power were brass imports, which usually were marginal runners. The joke was that if you modeled in HOn3, you also modeled in HO; not to demonstrate the difference in size but to have something to run! If you wanted rolling stock, you either scratchbuilt or assembled craft kits. The Schwedlers helped people successfully model narrow gauge. They carried kits, parts, and expertise on how to successfully accomplish any narrow gauge goal. For decades, Coronado has been the go-to place not just for advice, but for the supplies needed to create your narrow gauge dream. Though they specialized in On3 at first, they carry all scales and are superb modelers in any scale. They combine retail experience with modeling expertise, a rare commodity in a "hobby shop."

Stan and noted railroad artist, Jan Rons, attended college together and joke that they have survived all these years. So has Coronado. The Schwedlers have been an integral part of narrow gauge since the early 1960s and show no signs of slowing down. A visit to their shop is more than a hobby experience, it is also a reunion with old friends. Their sardonic humor and irreverent tone only enhance each encounter. Whether at a Narrow Gauge Convention or at 1544 E. Cypress Street, you will learn something about narrow gauge and the people who make it tick. Stan

and Sheldon note a shift in the narrow gauge scene from craftsman to ready-to-run. Some would argue, time has passed Coronado by. They would be wrong. Even ready-to-run sometimes doesn't. And not everything narrow gauge is ready-to-run. Stan and Sheldon have been around for almost 60 years ready to answer your questions, offer advice and provide that needed product for you. I sincerely hope they continue to do so for many years to come.

Well, that's all for now; until next time — write, if the mood strikes.

*Charles*

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# BOOK REVIEWS

Ron's Books, P.O. Box 714, Harrison, NY 10528, 914/967-7541, ronsbooks@aol.com, www.ronsbooks.com continues to sell new releases of reproductions of industrial catalogs from Silver Lake Images LLC, Manufacturer's Catalog Archive. Each catalog sells for \$35.00.

Railroad Cranes Volume 1 contains 150 pages of Burro Model 15 Cranes, Byers Shovels and Cranes, Lambert-National Steam Hoists, and the story of P&H Crane Construction. Each section contains technical information, parts photos, and scale plans of parts and complete cranes. There are numerous photos showing cranes at work in industrial sites. The steam hoist section shows donkey engines at work hooked up to hoists, and the last section details how cranes are constructed. Having built several cranes, I know this catalog will provide you with the information you need to add a crane to your roster.

Atlas Electric Locomotives is a gem. It's 100 pages are full of diagrams, and photos of small mining locomotives including storage battery locomotives, low-profile locomotives, and larger two truck steeple cab locomotives. Some of the low-profile models seem to be no more than a flatcar. Make an interesting model. Should fit right over a small power truck. The little tramming locomotives are also interesting. They were made small so they could get into tight places in mines and be moved up and down in caged elevators.

Fairbanks-Morse Locomotives Volume 1: Diesels has 50 pages divided into a

discussion of the Fairbanks-Morse Train Master: 2400 H.P. Diesel Locomotive and their use on the Lackawanna Railroad. No narrow gauge or short line in this catalog.

Heisler Locomotives Volume 2: Geared Locomotives is a 118 page, 5½-inch by 8½-inch catalog. So far this is the first small catalog I have reviewed. It is a reproduction of Heisler catalogs No. 108 and No. 115, with a 1908 Repair list showing parts numbers by list and diagram to aid in ordering repair parts. There are numerous diagrams showing how Heisler locomotives are constructed and many photos showing them at work in the woods, or at industrial sites. There are also many images of the Heisler factory and Heisler builder's photos. Bob Brown.

The Sandy River and Rangeley Lakes Railroad and Predecessors, The Railroad, Towns and Related Industries, Volume 7, The Towns of Kingfield, Carrabassett and Bigelow, by Jerry DeVos, 2022. Stonybrook Press, 4168 Hearthstone Dr., Sarasota, FL 34238, 941/882-4516, JDEVOS99@aol.com. Hardcover, illus., 281 pages, \$70.00 plus \$7.00 shipping, PayPal accepted.

This is the 7th in Jerry DeVos' monumental series on the 2-foot gauge Sandy River and Rangeley Lakes Railroad and its predecessors. It is organized geographically by milepost just like the preceding 6 volumes. The Table of Contents begins with a Dedication, followed by a Preface & Acknowledgments, Organization, Author's Comments, Corrections and Additions to Volumes 5 and 6, and a chapter on the Origins of the Franklin & Megantic and Kingfield & Dead River Railroads. Then it discusses Kingfield.

After a brief history of Kingfield, we learn about coal sheds, water tanks, water cranes, depots, freight stations, industries, people, spool factory, three stall engine house, and see photos of the parade celebrating Kingfield's Centennial. There are also prototype plans for a wood and a steel turntable. There is a list of employees who once kept Kingfield moving with photos and a description of the Great Race of 1905. The sawmills and novelty mills in the area are described one by one, with many photos.

The second section starts with a brief history and reorganization of the Kingfield & Dead River Railroad, followed by a description of the railroad milepost by milepost. The sawmills and bridges are shown along with a drawing of the Salem stream bridge. Again,

accidents are shown and discussed and there are maps and an aerial view of the area with details called out. Each crossing is called out and some drawings of bridge improvements are included.

The third and last section of the book covers Jerusalem Township and is the longest and most detailed section. Again, after a brief history of the area we learn about the water tanks, sidings, sheds, spurs, sluices, camps and see sketches for a bridge improvement, and again read about accidents along the line. The section house, trainman's house, and depot, throughout its history, are covered along with the engine house, sidings and wye including track plans over the years, and a great aerial view with callouts of the various details.

One section discusses operations between milepost 24 to 30 and has plans for modifications to the Huston Brook Bridge. One surprise to me was the description of a siding used by the S.D. Warren Paper Company of Portland, Maine. The company's 2-foot gauge paper plant railroad has always been a favorite of mine and was the subject of a special issue of Finelines many years ago. I was interested to see the company had its own siding for pulpwood at milepost 29. There are numerous photos of crews returning Forney #6 and her train to the track after she took a dive down an embankment.

The book ends with a Bibliography and Index. It is printed on high quality paper and includes many previously unpublished photos. I like the way the people of the area are shown riding the trains, taking photos of them, or working on the railroad. One especially delightful image shows Mary Prescott, a young girl serving as a Station Agent complete with cap badge, and the snapshot of two children sitting quietly enduring a snapshot in front of a steel bridge with stone piers.

It's hard to describe this fact-filled book, there is just so much. If you are a 2-foot historian or modeler, make sure you add this book to your library. Bob Brown.

# YOU TUBE VIDEO



## Layout Tour

I had the pleasure of having John Abatecola and his wife, Cydney, of TSG Multimedia video my layout recently. They were also helped by Dave Adams. I was pleased with the result and feel it really shows off my modeling and my layout.

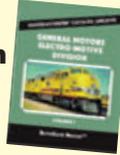
You can watch the video by going to TSG Multimedia You Tube and scrolling down to Tuolumne Forks. On3 Layout Tour With Bob Brown. I hope you enjoy the tour. Bob Brown.

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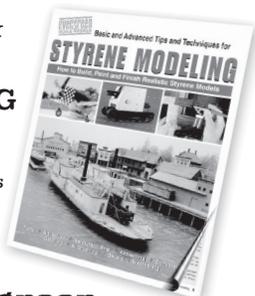
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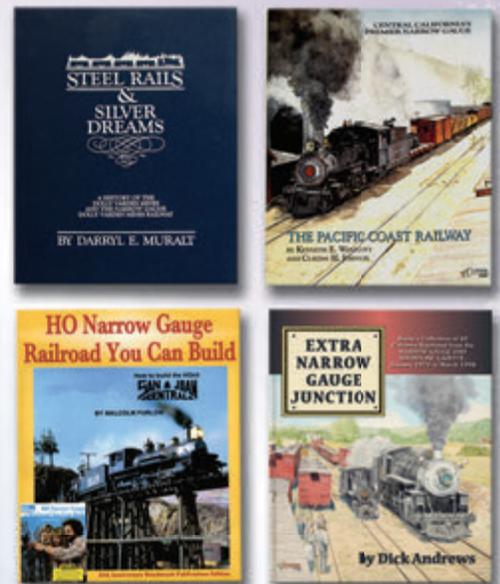
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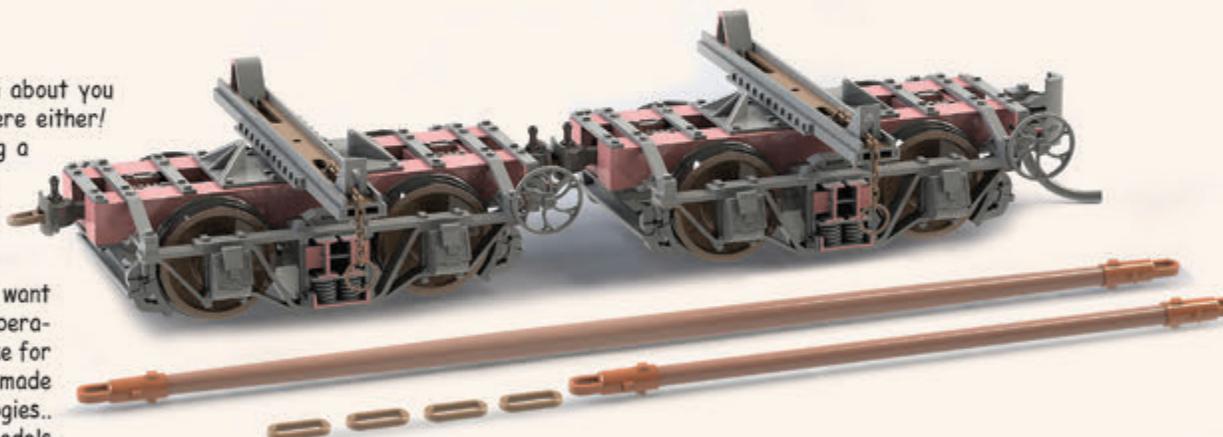
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